

VILLAGE OF ORLAND PARK

14700 Ravinia Avenue
Orland Park, IL 60462
www.orlandpark.org



Meeting Agenda

Tuesday, April 16, 2019

7:00 PM

Village Hall

Plan Commission

Nick Parisi, Chairman
Commissioners: Paul Aubin, John J. Paul,
Laura Murphy, Dave Shalabi
Edward Schussler and Patrick Zomparelli

Short Agenda Council Boiler

CALLED TO ORDER/ROLL CALL

APPROVAL OF MINUTES

[2019-0032](#) Minutes of the April 2, 2019 Plan Commission Meeting

Attachments: [04-2-19 meeting minutes](#)

PUBLIC HEARINGS

[2018-0619](#) Tommy's Car Wash - Development Petition for Site Plan, Landscape Plan, Elevations, Special Use Permit Amendment with Modifications

Attachments: [Site Plan](#)
[Building Elevations](#)
[Wolf Point Plaza Planned Development - 2010 Approved Plans](#)
[Building Elevations - Details and Material Specifications](#)
[Building Renderings - Colored Elevations](#)
[Landscape Plan](#)
[Low Wall Elevation](#)
[Special Use Responses](#)

BS

NON-PUBLIC HEARINGS

OTHER BUSINESS

[2019-0003](#) Solar Panel Installation - 88 Silo Ridge Road West

Withdrawn

[2019-0142](#) Memo: New Petitions & Appearance Review

Attachments: [4-16-19 Plan Commission Memo](#)

NON-SCHEDULED CITIZENS & VISITORS

ADJOURNMENT

DATE: March 5, 2019

REQUEST FOR ACTION REPORT

File Number: **2019-0032**
Orig. Department: **Development Services Department**
File Name: **Minutes of the April 2, 2019 Plan Commission Meeting**

BACKGROUND:

BUDGET IMPACT:

REQUESTED ACTION:

VILLAGE OF ORLAND PARK

14700 Ravinia Avenue
Orland Park, IL 60462
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Meeting Minutes

Tuesday, April 2, 2019

7:00 PM

Village Hall

Plan Commission

Nick Parisi, Chairman
Commissioners: Paul Aubin, John J. Paul,
Laura Murphy, Dave Shalabi
Edward Schussler and Patrick Zomparelli

CALLED TO ORDER/ROLL CALL

Present: 7 - Chairman Parisi; Member Aubin; Member Paul; Member Murphy; Member Shalabi; Member Schussler, Member Zomparelli

APPROVAL OF MINUTES

2019-0032 Minutes of the March 19, 2019 Plan Commission Meeting

APPROVED

Aye: 7 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Shalabi, Member Schussler and Member Zomparelli

Nay: 0

PUBLIC HEARINGS

OPEN PUBLIC HEARING

APPROVED

Aye: 7 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Shalabi, Member Schussler and Member Zomparelli

Nay: 0

2019-0083 Peony Place Subdivision - SP, VAR, SUB

Presentation by King, made in accordance with written staff report dated March 29, 2019. (refer to audio 06:15 – 15:59)

Discussion included flooding concerns and sidewalks. All Commissioners were in support of this project.(refer to audio 16:00 - 21:48)

Regarding Case Number 2019-0083, also known as Peony Place Subdivision, I move to accept as findings of fact of this Plan Commission the findings of fact set forth in this staff report, dated April 2, 2019;

and

I move to recommend to the Village Board of Trustees to approve the preliminary site plan titled “Preliminary Site Plan”, prepared by DesignTek Engineering, LLC and dated March 11, 2019 and last revised March 22, 2019, subject to the following conditions:

1. Submit a Landscape Plan that includes parkway tree requirements and a tree mitigation plan before this project goes before the Village Board of Trustees, and;
2. Install permeable pavement in the driveways of all three lots, and;
3. Meet all final engineering requirements including required permits and approvals from outside agencies, and;
4. Meet all building code requirements, and;

5. Screen all mechanical equipment either at grade or on the rooftop;

and

I move to recommend to the Village Board of Trustees to approve the preliminary plat of subdivision, titled, "Final Plat of Peony Place", prepared by DesignTek Engineering, LLC and dated January 3, 2019 and last revised March 19, 2019 for 14421 Second Avenue and 9852 144th Place into three single family lots subject to the same conditions outlined in the above preliminary site plan and the following conditions:

1. Submit a Record Plat of Subdivision to the Village for execution and recording;

and

I move to recommend to the Village Board of Trustees to approve a variance for Lots 1 & 2 of the Peony Place subdivision to increase the maximum lot width from 50 feet to 62.5 feet as depicted in the above referenced site plan;

and

I move to recommend to the Village Board of Trustees to approve a variance for the Peony Place subdivision to waive the required sidewalk along the east side of Second Avenue and the west side of Third Avenue as depicted in the above referenced site plan.

APPROVED

Aye: 7 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Shalabi, Member Schussler and Member Zomparelli

Nay: 0

CLOSE PUBLIC HEARING

APPROVED

Aye: 7 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Shalabi, Member Schussler and Member Zomparelli

Nay: 0

OPEN PUBLIC HEARING

APPROVED

Aye: 7 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Shalabi, Member Schussler and Member Zomparelli

Nay: 0

2018-0458 Hawkeye Hotels - Hampton Inn

Presentation by Mazza, made in accordance with written staff report dated March 29, 2019. (refer to audio 25:53 – 36:01)

Discussion included concerns about traffic, parking, property values, landscaping and child safety for the daycare next door. The petitioner assured the public that it is not an extended-stay hotel, and spoke about the hotels customers, which includes primarily businessmen and families. (refer to audio 36:40 – 2:27:42)

I move to accept as findings of fact of this Plan Commission the findings of fact set forth in this staff report, dated April 2, 2019;

And

I move to recommend to the Village Board of Trustees to approve the site plan as shown on “Preliminary Site Plan”, prepared by Advantage Consulting Engineers, dated 05/02/2018, last revised 03/10/19, sheet SP1, subject to the following condition:

- 1) Meet all final engineering and building code requirements;
- 2) Update all project plans, data tables and documents to adjust the proposed number of hotel rooms from one hundred and fifty-three (153) to one hundred and fifty-eight (158).

And

I move to recommend to the Village Board of Trustees to approve the elevation drawings titled “Hampton Inn, Orland Park”, Sheets A1.2, A2.2, A3.1, A3.2 and A4.1, prepared by Base4, dated 01/10/2019, subject to the following conditions:

- 1) Screen all mechanical equipment either at grade or at rooftop with landscaping or parapets respectively;
- 2) Masonry must be of anchored veneer type with a minimum thickness of 2.625 inches;
- 3) Signs are subject to additional review and approval via the sign permitting process;
- 4) Meet all building code requirements;
- 5) Additional screening may be required upon further project review.

And

I move to recommend to the Village Board of Trustees to approve the preliminary

landscape plan titled "Landscape Plan", prepared by Allied Nurseries, dated 03/01/2019, sheets L-1, L-2, and L-3, subject to the following condition:

1) Submit a final landscape plan and all required supporting documentation in coordination with final engineering submittal.

And

I move to recommend to the Village Board of Trustees to approve a Special Use Permit amendment with modifications for Main Street Village West to allow for a hotel greater than 50,000 square feet in size, subject to the same conditions as outlined in the Preliminary Site Plan motion. Modifications to the Special Use permit include:

1) A modification to locate parking and a garbage enclosure in the setback between the building and the street;

2) A modification to locate off-street parking in the setback between the building and the street;

3) A modification to locate a storage shed is proposed in the side yard;

4) A modification to increase the number of permitted parking spaces by forty-one (41) spaces from one hundred and fifty-eight (158) to one hundred and ninety-nine (199);

5) A modification to reduce the number of required loading spaces from four (4) to one (1);

6) A modification to reduce the minimum parking stall depth from eighteen (18) to approximately seventeen (17) feet and the minimum drive aisle width from twenty-two (22) to approximately twenty-one (21) feet.

And

I move to recommend to the Village Board of Trustees to approve the preliminary plat of consolidation titled "Plat of Consolidation - 16160 S. LaGrange Road", prepared by JLH Land Surveying, Inc., dated 02/18/2019 subject to the following conditions:

1) Submit a final Record Plat of Subdivision to the Village for execution and recording;

2) All dedications, jurisdictions and document numbers must be referenced on the plat for any necessary right of way dedications.

APPROVED

Aye: 6 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Schussler and Member Zomparelli

Nay: 0

Absent: 1 - Member Shalabi

CLOSE PUBLIC HEARING

APPROVED

Aye: 6 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Schussler and Member Zomparelli

Nay: 0

Absent: 1 - Member Shalabi

OPEN PUBLIC HEARING

APPROVED

Aye: 6 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Schussler and Member Zomparelli

Nay: 0

Absent: 1 - Member Shalabi

2019-0119 Seritage - Orland Square Mall - Sears Redevelopment REVISED 2019

Presentation by Turley, made in accordance with written staff report dated March 29, 2019. (refer to audio 2:41:25 – 3:00:11)

Discussion included parking, sidewalks and loading dock. All Commissioners were in support of this project. (refer to audio 3:02:55 – 3:10:03)

I move to accept as findings of fact of this Plan Commission the findings of fact set forth in this staff report, dated April 2, 2019.

And

I move to recommend to the Village Board approval of the Preliminary Site Plan titled 'Seritage Redevelopment - Orland Square Site Plan', prepared by RA Smith Inc, sheet C200, dated 01.10.19, revised 03-19-19 ; and titled 'Proposed Floor plan' by Hobbs and Black Architects, dated 01.10.19, revised 03.19.19, sheet A-1, subject to the following conditions:

- 1) Meet Village requirements for parking lot pavement improvements as determined by Village inspection post-construction.
- 2) Repair concrete stairwell that connects upper to lower parking lots and include decorative safety lighting.
- 3) Work with Orland Fire to relocate Auto Center Fire Department connection

prior to demolition.

4) Meet all final Engineering and Building Division requirements and approvals.

And

I move to recommend to the Village Board of Trustees approval of the Preliminary Landscape Plan titled 'Seritage Redevelopment - Orland Square Landscape Plan', prepared by RA Smith, Inc, and dated 01.10.19, sheets L100-L106, subject to the following conditions:

1) Meet conditions outlined in the most recent Hey and Associates review letter dated February 13, 2019, and all subsequent review letters.

2) Submit a Final Landscape Plan for Village approvals after Final Engineering approval.

I move to recommend to the Village Board approval of the Elevations titled 'Orland Square Sears Redevelopment - Elevations' prepared by 5o5 Design, sheets 1-14, dated 03.13.19; and as titled "Orland Square Proposed Elevations", prepared by Hobbs + Black Architects, sheet AD-1 and AD-2, dated 03.15.19, subject to the to the following conditions. All changes must be made and conditions met prior to the Board meeting.

1) All service doors and safety railings must match the color of the abutting building façade, unless otherwise regulated by Building and Fire Codes.

2) Screen all new mechanical equipment either at grade level with landscaping or hidden behind the roofline.

3) Meet all final Engineering and Building Division requirements.

4) All masonry must be of anchored veneer type masonry with a 2.625" minimum thickness.

5) Remove Directory Wall signs from the building elevations since they are not permitted by Code. Signs are subject to additional review and approval via the sign permitting process and additional restrictions will apply. Electronic message board signs are also not allowed.

And

I move to recommend to the Village Board approval of a Special Use Permit Amendment for the Orland Square Mall Planned Development to allow construction and operation of a movie theater and for a commercial development with restaurants that will continue to exceed 50,000 square feet, subject to the same conditions as outlined in above motions.

And

I move to recommend to the Village Board approval of the following Code Modifications for Seritage - Orland Square Mall / Sears Redevelopment petition project subject to the same conditions as outlined in above motions:

- 1) Exceed lot coverage.
- 2) Reduce landscaping requirements for buffers and foundation plantings.
- 3) Reduce parking setbacks and allow parking and drives located between the building and the street.
- 4) Reduce side and rear building setbacks.

All conditions must be met and changes made prior to the Board meeting.

APPROVED

Aye: 6 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Schussler and Member Zomparelli

Nay: 0

Absent: 1 - Member Shalabi

CLOSE PUBLIC HEARING

APPROVED

Aye: 6 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Schussler and Member Zomparelli

Nay: 0

Absent: 1 - Member Shalabi

NON-PUBLIC HEARINGS

2018-0499 Orland Ridge - Planned Development

No discussion.

I move to approve the Preliminary Site Plan, Park Area Exhibit, Elevations, Preliminary Landscape Plan, Subdivision with Preliminary Plat, a Special Use Permit for a Planned Development for Orland Ridge, and additional Special Use Permits, as recommended at the March 18, 2019, Development Services, Planning and Engineering Committee meeting and as noted in the below fully referenced motion:

THIS SECTION FOR REFERENCE ONLY (NOT NECESSARY TO BE READ)

I move to approve the preliminary site plan titled "Site Dimension Plan", prepared by Kimley Horn & Associates & Associates, dated July 13, 2019, last revised March 8, 2019, sheets C2.0 and C2.1 and the park exhibit titled "Park Area Exhibit", prepared by Kimley Horn & Associates, dated November 6, 2018 and

revised November 16, 2018, subject to the following conditions:

1. The commercial component of the preliminary site plan is considered conceptual, with the exception of the proposed community gathering space, located between Building B and Building C, and will need to go back through the entire Development Review process and approval; once tenants are selected and the project is designed. The community gathering space is not conceptual and is required, as a condition of the site plan approval; and
2. An additional community gathering space that measures, .29 Acres, in the commercial area will be required, as a condition of the site plan approval; and
3. The hotel component of the preliminary site plan is considered conceptual and will need to go back through the entire Development Review process and approval; once tenants are selected and the project is designed. Future plans for the hotel parcel must provide two (2) vehicular ingress/egress points; and
4. 169th Place be fully constructed including sidewalks and multi-use paths, and connected to La Grange Road prior to final occupancy being granted for more than 50% of the residential units; and
5. The developer will be responsible for the maintenance of the landscaping areas and the roundabout along 169th Place and shall work with the Village to prepare a maintenance agreement for the landscaping areas along 169th Place; and
6. With the exception of the accessory structure modifications that have been requested, all accessory structure and uses must meet standard Land Development Code requirements, Section 6-302; and
7. All private park space, with the exception of the clubhouse and pool area, must be publicly accessible to the general public; signage must be installed that indicates the public accessibility. The development may not be gated nor outside public access otherwise restricted. The clubhouse must be equally available to all residents of the development; and
8. Based on proposed park land acreage and proposed park capital improvements; the developer will pay cash-in-lieu to the Village in the amount of \$76,371, based on the formula required by Village code, for the ½ acre shortage of required park land contribution and the developer will get the maximum credit of \$381,832 from the Village for the required cash donation for capital improvements. The developer will also pay \$100,000 towards a public art element for the project. The developer is required to work with staff on the selection, procurement and installation of the required public art.
9. Meet building code requirements; and

10. Meet all final engineering requirements including required permits from outside agencies; and
 11. Screen all mechanical equipment either at grade or at rooftop with landscaping or parapets respectively; and
 12. Submit a sign permit for all proposed signage; and
- and

I move to approve the elevation drawings titled "Preliminary Architectural Plans", compiled by SR Jacobson and Lormax Stern, dated February 6, 2019, with Ranch Villa Drawings prepared by Coponen Architects, Sheets AR-2, Typical Building Front Elevation only (dated 10/25/2018), A-4, Unit End Elevation, Typical Unit Rear Elevation, and the Typical Unit Front Elevation (dated June 13, 2018), AR-3 (dated December 18, 2018), Villa Front Elevation (dated 07/08/2018) and Villa Rear Elevation (Submitted 02/06/2019), Elevation A (Submitted 02/06/2019), Elevation B (Submitted 02/06/2019), and Elevation C (Submitted 02/06/2019); and also Townhome Drawings, prepared by Coponen Architects, Sheets A2 (dated 10/25/2018, revised 01/05/2019), and the Townhome Rendering (dated 11/03/2018); and also Clubhouse Drawings prepared by Alexander V. Bogaerts + Associates, P.C., Sheets 4 (dated 11/12/2018, s.p.a. 01/02/2019), and 5 (dated 11/12/2018, s.p.a. 01/02/2019), Rendered Elevation Sheet 4 (dated 11/12/2018, s.p.a. 12/21/2018), Rendered Elevation Sheet 5 (dated 11/12/2018, s.p.a. 12/21/2018); and also three (3) Commercial Plaza Renderings (dated January 2019), subject to the following conditions;

1. In the case of a conflict between exhibit drawings; the stricter application will apply;

and

I move to approve the preliminary landscape plan titled "Preliminary Landscape Plan", prepared by Kimley-Horn and Associates, Inc., dated July 13, 2019, last revised February 6, 2019, sheets L1.0 through L4.6, subject to the same conditions outlined in the above preliminary site plan and elevation motions and the following:

1. The Petitioner shall submit a final landscape plan and all required supporting documentation addressing all outstanding landscape items in coordination with final engineering submittal; and
2. The Petitioner shall install permanent "no mow" markers (such as bollards, posts, etc), at the back corner of every other property that abuts a stormwater

basin or wetland area to clearly indicate the 25' setback line; and

3. The Petitioner shall install environmental educational signage around all wetlands and detention ponds; and
4. The installation and maintenance of landscaping in all storm water management and wetland areas shall be performed by a qualified landscape contractor, as approved by the Development Services Department; and
5. Any trees or large shrubs in conflict with the petroleum gas line easement along 94th Avenue should be relocated elsewhere on site;

and

I move to approve the preliminary plat of subdivision titled "Orland Ridge", prepared by Landmark, subject to the same conditions outlined in the above preliminary site plan and elevation motions and the following:

1. Prior to the Development Committee meeting, change the title of the plat to "Preliminary Plat of Subdivision - Orland Ridge" and provide a date on the plat; and
2. Submit a Record Plat of Subdivision to the Village for execution and recording; and
3. All dedications, jurisdictions and document numbers must be referenced on the plat for any necessary right of way dedications;

and

I move to approve the following Special Use Permits for the Orland Ridge Planned Development, subject to the same conditions outlined in the above preliminary site plan, landscape plan and elevation motions. Modifications to the Special Use Permits include:

1. A Special Use Permit for a Planned Development with multiple buildings and multiple uses:
 - a. With modifications to reduce building setbacks for the townhomes:
 - i. Front building setback from collector streets - COR Mixed Use District, Section 6-2100F.1.a.(1)(b). (From 40' to 25')
 - ii. Front building setback from local street - COR Mixed use District, Section 6-210 F.1.a. (1)(c) (From 30' to 20')
 - iii. Building to building side setback (From 30' to allow for a minimum building to building separation of 15') COR Mixed use District, Section 6-210 F.2.
 - iv. Building side to local street setback (From 30' to 20') COR Mixed Use District,

- Section 6-210 F.1.a. (3) (c)
- v. Building corner side to collector street setback (From 40' to 25') Mixed Use District, Section 6-210 F.1.a (3) (b).
 - vi. Balcony encroachment into front setback (From 3' limit to 5.5' maximum) Accessory Structures and Uses Section 6-302 C. 4.
- b. With modifications to reduce building setbacks for the Villas:
- i. Building side to side setback from (From 30' to allow for a minimum building to building separation of 20') COR Mixed use District Section 6-210 F.2.
 - ii. Building corner side to collector street setback (From 40' to 25') COR Mixed use District Section 6-210 F.1.a. (3) (b).
 - iii. Building side to local street setback (From 30' to 25') COR Mixed use District Section 6-210 F.1.a (3) (c).
 - iv. Building to building rear setback (From 60' to allow for a minimum building to building separation of 27') COR Mixed use District Section 6-210 F. 2.
- c. With a modification to reduce the setback from the detention pond high water line Storm Sewers and Storm Water Retention Section 6-409 E. 18.0 (From 25' to as little as 0')
- d. With a modification to exceed retaining wall height at overlook. Accessory Structures and Uses Section 6-302 C.31. (From 3' to 6')
- e. With a modification to reduce Off-Street parking and loading requirements, Section 6-302 including Section 6-302 E.7.a., which allows driveway parking to count toward parking requirements only if two off-site spaces are provided elsewhere on the site.
- f. With a modification to allow lot coverage to be calculated as an aggregate of all the parcels within the Planned Development in the COR Mixed use District, Section 6-210 G.
- g. With a modification to allow air conditioning units in the front and side setbacks of the Townhomes and Villas, Section 6-302 C.1.
- h. With a modification to reduce parkways, Section 6-406.2.a. and 3.
- i. With a modification to increase detention pond release rates to a rate that will protect the existing regulatory wetland hydrology, in accordance with Federal regulations, Section 6-409.E.18.c.
2. A Special Use Permit to allow attached dwellings in the COR Mixed use District, Section 6-210. C.2.
3. A Special Use Permit for a site plan with total building area greater than 50,000 square feet, Section 6-210.C.23.
4. A Special Use Permit to allow residential uses to exceed more than 40% of the Mixed Use Planned Development in the COR Mixed use District, Section 6-210.C.2.B.

- 5. A Special Use Permit to construct a private park and community center, Section 6-210.C.6,17:
 - a. With a modification to increase the maximum private park acreage from 2 acres to 3.75 acres, Section 5-112.H.2.c

- 6. A Special Use Permit for the disturbance of wetlands, Section 6-413:
 - a. With a modification to eliminate two small wetlands.
 - b. With a modification to reduce the 50' wetland setback for the remaining preserved wetland.

OTHER BUSINESS

None.

2019-0142 Memo: New Petitions & Appearance Review

NON-SCHEDULED CITIZENS & VISITORS

ADJOURNMENT

A motion was made by Chairman Parisi, seconded by Commissioner Zomparelli, to adjourn the meeting at 10:15pm

ADJOURNED

Aye: 6 - Chairman Parisi, Member Aubin, Member Paul, Member Murphy, Member Schussler and Member Zomparelli

Nay: 0

Absent: 1 - Member Shalabi

Respectfully Submitted,

Jennifer Sexton
Recording Secretary

REQUEST FOR ACTION REPORT

File Number: **2018-0619**
Orig. Department: **Development Services Department**
File Name: **Tommy's Car Wash - Development Petition for Site Plan, Landscape Plan, Elevations, Special Use Permit Amendment with Modifications**

BACKGROUND:

Project

Tommy's Car Wash - 15819 Wolf Road
2018-0619

Petitioner

Daniel Elliot

Purpose

The petitioner seeks approval to construct a 5,167 square foot car wash building on a 1.54 acre parcel.

Requested Actions: Site Plan, Elevations, Landscape Plan, and Special Use Permit Amendment with Modifications

Address: 15819 Wolf Road

P.I.N.: 27-17-315-001-0000

Parcel Size: 1.54 acres

Comprehensive Plan Planning District & Designation: Centennial Planning District with Neighborhood Mixed Use Designation

Existing Zoning: BIZ General Business District

Existing Land Use: Vacant

Proposed Land Use: Car wash facility

Surrounding Land Uses & Zoning:

North: BIZ General Business District - Vacant lot (Wolf Point Plaza shopping center)

South: RSB Residential and Supporting Business District - Bank (across 159th Street)

East: BIZ General Business District - Vacant lot (Wolf Point Plaza shopping center)

West: BIZ General Business District - Commercial strip center (across Wolf Road)

Preliminary Approvals: Preliminary engineering and landscape plan approvals have been granted for this project to move forward to the Plan Commission.

BACKGROUND & CONTEXT

The subject property is located on Lot 1 of Wolf Point Plaza. On April 5, 2010, the Village Board approved a Special Use Permit to allow for a Planned Development for Wolf Point Plaza by Ordinance No. 4558 (Legistar File ID # 2008-0422). Wolf Point Plaza consists of five (5) lots on 18.12 acres located at the northeast corner of 159th Street and Wolf Road. Lot 1, 2, and 4 were originally envisioned for retail, restaurant, and bank users, each with drive-through, and are currently vacant. The subject property, Lot 1, was originally approved for a CVS pharmacy with parking and a drive-through lane in the setback areas between the building and the street. The BMW of Orland Park car dealership is located on Lot 3 at 11030 W. 159th Street. Lot 5 includes wetlands and a shared detention pond for the entire Planned Development. The original Special Use Permit included several modifications and variances for

the Wolf Point Plaza development. The Special Use Permit has been amend twice times allow for additional parking, increased lighting levels, and a 40' flag pole for BMW (Ordinance No. 4681-11/7/2011) and a building addition for BMW (Ordinance No. 5096 - 5/16/18).

PROJECT DESCRIPTION

The petitioner is proposing to construct a 5,167 square foot car wash building on a 1.54 acre parcel. The site is located at 15819 Wolf Road at the northeast corner of 159th Street and Wolf Road. The proposed site plan consists of twenty-two (22) parking spaces, drive-through lanes, a pay station canopy, vacuum station canopies, and landscape enhancements. The project will also include two (2) access drives from Zeigler Drive, a private road to the north, and two (2) cross access drives to the adjacent lot to the east.

The project requires approval of an Amendment to the Special Use Permit for Wolf Point Plaza (Ordinance No. 4558). Additionally, car washes are classified as motor vehicle services, which require approval of a Special Use Permit in the BIZ General Business District. The petitioner requests approval of the following modifications:

1. A modification to locate drive-through lanes in the setback areas between the building façade and the street (Section 6-207.F.4)
2. A modification to increase the number of drive-through lanes between the building façade and the street from one (1) lane with a maximum width of fourteen (14) feet to three (3) lanes with a combined width of thirty-six and one-half (36.5) feet (Section 6-207.F.4)
3. A modification to Section 6-306.B to increase the number of parking spaces from two (2) spaces to twenty-two (22) spaces (Section 6-306.B)

With the exception of the requested modifications, the project conforms to the Village's Comprehensive Plan, Land Development Codes and policies for this area.

SITE PLAN

The site plan consists of a 5,167 square foot car wash building, a parking lot with sixteen (16) vacuum station canopies, two (2) cross access drives connecting to the adjacent property to the east, and new landscaped areas. The proposed three-lane drive-through will extend around west side of the building and merge into a single drive-through lane to the south of the building.

MOBILITY

Vehicular/Traffic

The site will be accessed from two (2) curb cuts off of Ziegler Drive, a private street to the north of the subject property that serves the Wolf Point Plaza development. The west driveway will be an entrance only and the east driveway will be full access consisting of one outbound left turn lane, one outbound right turn lane, and one inbound lane. Ziegler Drive connects to Wolf Road, a minor arterial street under IDOT jurisdiction, at a right-in/right-out intersection. The property is also located directly to the north of 159th Street, a major arterial street under IDOT jurisdiction, but will not have direct access to this street. Vehicles will instead be able to enter from the right-in/right-out intersection on 159th Street and Dealership Drive further to the east.

Cross-Access

Two (2) cross access drives are proposed to connect to future development on the adjacent property to the east. The petitioner will be required to submit a final plat during final engineering establishing a new cross access easement.

Drive-Through Lanes

Per Section 6-207.F.4, drive-through facilities are not allowed within the setback area between the building facade and the street unless approved via a modification to a Special Use Permit. In order to qualify for a Special Use Modification, the drive-through facility must meet all of the conditions in Section 6-207.F.4. The code states that drive-through should be designed as a single lane with a maximum width of 14' and a minimum turning radius of 20' from the centerline of the drive aisle. Drive-through lanes must be a minimum of 10' from all property lines, constructed of decorative pavement or pervious pavers, and screened with a combination of masonry walls and/or decorative landscaping in order to provide a solid

buffer at a minimum height of 36". The drop off area or drive aisle shall not obstruct any direct pedestrian connections and shall include crosswalks. Order windows and menu boxes must be integrated into the architecture of the building, include masonry architectural details that match the materials and design of the building, screened from vehicular and pedestrian view, and kept out of the setback between the building and the street.

A modification is requested to allow for a three-lane drive-through between the building and Wolf Road. The proposed drive-through lane meets most of the conditions per Section 6-207.F.4 but exceeds the allowed lane width size. As a result, the petitioner has also requested a modification to increase the number of drive-through lanes between the building façade and the street from one (1) lane with a maximum width of fourteen (14) feet to three (3) lanes with a combined width of thirty-six and one-half (36.5) feet.

As shown on the submitted site plan, the proposed drive-through will be constructed of colored concrete with a scored diamond crosshatched pattern. A different color of decorative concrete will be used between the drive-through lane islands. The landscape area between the drive-through lanes and Wolf Road will include a bioswale with naturalized plantings, a berm measuring approximately four (4) feet in height above the grade of the drive-through lanes, and a mix of shrubs, shade trees, and ornamental trees. A masonry low wall surrounded by landscaping will also be located at the southwest corner of the site.

A modification is also requested to allow for a single drive-through lane and emergency exit lane between the building and 159th Street. The lanes will be constructed of colored concrete with a diagonal diamond crosshatched pattern. As proposed, the single drive-through lane meets the conditions of Section 6-207.F.4 in terms of the design, setbacks, use of decorative pavement, and screening and landscaping requirements.

Parking

Required: Two (2) spaces, including one (1) accessible space

Provided: Twenty-two (22) spaces, including one (1) standard accessible space and one (1) vacuum accessible space

Per Section 6-306.B, car washes are also required to provide three (3) stacking spaces per drive-through lane plus one (1) parking space per employee. In this case, two (2) parking spaces are required for the two (2) proposed employees at peak shift and a total of nine (9) stacking spaces are required for the three (3) drive-through lanes.

A total of twenty-two (22) parking spaces are proposed, which includes two (2) standard parking spaces, three (3) parallel parking spaces, fifteen (15) vacuum spaces measuring fourteen (14) feet wide, one (1) standard accessible space, and one (1) accessible vacuum space. In total, sixteen (16) of the parking spaces will be served by vacuums. Any request exceeding a twenty percent (20%) increase in the total number of parking spaces permitted on the lot must be approved through a modification to the Special Use Permit. The petitioner has requested a modification to Section 6-306.B. to allow for additional parking spaces associated with the vacuum stations.

A total of twenty-four (24) stacking spaces are located within the three drive-through lanes between the payment canopy and the adjacent drive aisle, which meets and exceeds code requirements. An additional five (5) stacking spaces are located in the single lane drive-through area between the payment canopy and the car wash building entrance.

Five (5) bicycle parking spaces are proposed adjacent to the sidewalk at the northwest corner of the building, which meets code requirements per Section 6-306.H.

Pedestrian Access

A five (5) foot wide sidewalk is proposed along the Ziegler Drive along the north of the site. The sidewalk will connect to the existing public sidewalk along Wolf Road and to future development on the vacant lot

to the east. The sidewalk system will extend into the interior of the site to connect to the building entrances. Decorative stamped concrete is proposed where the sidewalk cuts through the parking lot/access drive to create a defined pedestrian route. Additionally, a sidewalk section will be installed in front of the proposed low wall at the southwest corner of the property to connect the public sidewalks along Wolf Road and 159th Street.

BUILDING ELEVATIONS

The proposed one-story building will be constructed of gray brick, 4 inch split faced concrete masonry block, a curved acrylic roof system, and aluminum metal accent panels and banding. The proposed building materials and color scheme are included on the elevations, specification sheets, and renderings submitted by the petitioner.

North and South Elevations

The north and south elevations of the building will each be accentuated by red and black aluminum panels surrounding the car wash entrance and exit, a curved acrylic roof system and fascia, and a brick accent tower featuring a 4 inch split face concrete masonry block base in a dark gray color, an accent band composed of gray corrugated metal lined with black metal banding on the top and bottom, and an aluminum parapet cornice. The accent tower on the south elevation near 159th Street has a proposed height of 28 feet measured to the top of the parapet while the accent tower on the north elevation near Ziegler Drive has an overall height of 24 feet. The parapet walls shall fully screen any proposed mechanical equipment to meet code requirements listed in Land Development Code Section 3-608.J.

East and West Elevations

The east and west elevations utilize the same architectural style and materials used on the north and south elevations. The central portion of the building features a curved acrylic roof, large glass windows, and louvers. The proposed building height to the top of the car wash tunnel measures 20 feet 9 inches.

Each end of the west elevation facing 159th Street will be constructed of gray brick with a gray corrugated metal accent band with black metal banding. The central portion of the elevation includes a curtain wall with large transparent windows that will be topped with horizontal louvers, the curved acrylic roof system, and a shielded red LED accent light. The payment canopy and drive-through window serving the three-lane drive-through will be located on the west elevation.

The east elevation also consists of the curved acrylic roof system and a curtain wall with large transparent windows topped with horizontal louvers. Two (2) of the windows will consist of black, non-transparent metal to block views of interior operations inside the car wash building.

Additional information on the proposed elevations of the proposed payment canopy, vacuum station canopies, vacuum equipment enclosure, and garbage enclosure are included in the Accessory Structure section below.

LANDSCAPE PLAN

The preliminary landscape plan has been approved by the Village's landscape consultant, Hey & Associates. The petitioner must submit a final landscape for separate review in conjunction with final engineering. All conditions included in the most recent and subsequent comment letters must be addressed during final landscape plan review. It is imperative that the landscape design professional and project engineer coordinate their work to provide grading required for landscape compliance.

Overall, the landscaping requirements for Landscape Parkways, Landscape Corridors, Landscape Bufferyards, and Foundation and Interior Landscape, Parking Lot Area Landscape, and Signage Landscape have been met and will be confirmed during final landscape plan review. As previously noted, landscaping is proposed to help screen the drive-through lanes from view and offset the visual impacts associated with the requested modifications to Section 6-207.F.4 of the Land Development Code. The petitioner has provided a landscape area between the three drive-through lanes and Wolf Road that includes a bioswale with naturalized plantings, a berm measuring approximately four (4) feet in height above the grade of the drive-through lanes, and a mix of shrubs, shade trees, and ornamental trees.

Shrubs and perennials are proposed in front of the single drive-through lane along 159th Street to meet code requirements. Additionally, a curved masonry wall is proposed at the southwest corner of the site, which will be surrounded by a mix of deciduous and evergreen shrubs, perennials, and ornamental grasses. Please refer to the Accessory Structures section below for additional details on the low masonry wall.

A small bioswale is also proposed along the east side of the site. Stormwater Management Area Landscape must be revised to meet the requirements of Section 6-305.D.8. A monitoring and management (M&M) plan will be required at the time of final landscape review.

DETAILED PLANNING DISCUSSION

Preliminary Engineering

Preliminary engineering approval has been granted for this project by the Village's engineering consultant, Christopher B. Burke Engineering Ltd. (CBBEL). Final engineering submittal must address all previous review letter comments as well as any future engineering comments.

Detention/Retention - An existing shared detention pond was constructed on Lot 5 as part of the Wolf Point Plaza Planned Development to accommodate storm water and detention for the entire development. Two (2) bioswales are proposed on site.

Traffic Study - The petitioner has submitted a traffic study for this project, which is required for all proposed developments with drive-through facilities per Section 6-405. Based on information provided by Tommy's Car Wash Systems, the 130 foot long car wash tunnel can process an estimated 130 vehicles per hour, resulting in average of one vehicle exiting the car wash building every 28 seconds. Because the car wash tunnel allows multiple vehicles to go through the car wash simultaneously (up to five vehicles at the same time), the tunnel can accommodate more than one vehicle every 28 seconds.

It is anticipated that the storage for thirty-one (31) vehicles from the entrance of the car wash building to the north driveway will accommodate the peak queuing demands. In the event that vehicles stack into Ziegler Drive during extreme peak times of operation, a high demand contingency plan will be implemented where an employee will temporarily close off the west driveway entrance and direct traffic to the east driveway. This would provide stacking for an additional four (4) vehicles on site, bringing the total number of stacked vehicles to thirty-five (35) including the stacking space between the payment canopy and the car wash building entrance. Based on the results of the analysis, no mitigation is needed at the study area intersections at this time and the proposed configuration of the driveways is adequate to serve peak hour traffic.

Lighting - A photometric plan will be reviewed during final engineering and shall meet the requirements of Section 6-315. In this case, Lighting Class 2 would be required for the project. Where non-residential is proposed adjacent to non-residential, 5 foot candles are allowed at the property line and a maximum light level of 15 foot candles is allowed within the interior of the site.

Comprehensive Plan

According to the Village's Comprehensive Plan, the subject property is located in the Centennial Planning District and is designated Neighborhood Mixed Use. The property is also identified as a Development Opportunity site. Within the Neighborhood Mixed Use Designation, drive-through facilities are considered conditionally appropriate.

Land Use/Compatibility

The proposed car wash is compatible with the BIZ General Business District, nearby existing uses along 159th Street, and adjacent automobile oriented uses. The surrounding area is comprised of a mix of established commercial uses, which includes a car dealership (BMW), banks, retail establishments, restaurants, and office and service uses. The vacant lots directly to the north and the east of the subject property are part of the Wolf Point Plaza Planned Development and were originally envisioned for restaurants and a bank with drive-throughs. The proposed car wash will be complementary to the BMW dealership to the northeast in Wolf Point Plaza. A strip shopping center is located to the west of the site

across Wolf Road. The properties to the south across 159th Street and the southwest of the site across 159th Street and Wolf Road are each occupied by a bank with a drive-through.

Special Use Permit

A Special Use Permit for a Planned Development for Wolf Point Plaza was approved by Ordinance No. 4558 on April 5, 2010 to allow for the development of an 18.12 acre commercial center with four retail buildings with drive-throughs and a car dealership. The proposed car wash will be located on Lot 1 of Wolf Point Plaza, which was originally approved for a CVS pharmacy with parking and a drive-through lane in the street setback. Car washes are classified as motor vehicles services, which require approval of a Special Use Permit in the BIZ General Business District per Section 6-207.C.12 of the Land Development Code.

Per Section 5-105.E. of the Land Development Code, the Plan Commission shall consider the extent to which the proposed project will meet the eight Special Use standards listed in the Code. The petitioner has provided responses to the Special Use standards, which are attached for review.

Modifications

As part of the Special Use Permit, the petitioner is requesting three (3) modifications to the Land Development Code, which are discussed in the responses to the Special Use Standards submitted by the petitioner. The requested modifications are listed below:

1. A modification to locate drive-through lanes in the setback areas between the building façade and the street (Section 6-207.F.4)

The petitioner has requested that a three-lane drive-through be located between the building and Wolf Road. A single drive-through lane is proposed between the building and 159th Street. Per the petitioner, the proposed site design and orientation of the drive-through lanes is "vital to the property operation of the facility."

The project is required to meet all of the conditions listed in Section 6-207.F.4 in order to qualify for a modification to allow for a drive-through lane between the building and the street. Landscaping, a berm, and a low masonry wall are proposed to screen the drive-through lanes and to offset the visual impacts associated with the proposed modification. The drive-through lanes meet setback requirements and will be constructed of decorative pavement. All of the drive-through standards per Section 6-207.F.4 have been met with the exception of the number of lanes and the width of the lanes allowed per code, which is discussed in the further detail below.

2. A modification to increase the number of drive-through lanes between the building façade and the street from one (1) lane with a maximum width of fourteen (14) feet to three (3) lanes with a combined width of thirty-six and one-half (36.5) feet (Section 6-207.F.4)

A modification is requested to allow for a three-lane drive-through in the setback area between the building and Wolf Road. As noted above, the proposed drive-through lane meets the majority of the conditions listed in Section 6-207.F.4 but exceeds the maximum width and number of lanes allowed per the Land Development Code. A single drive-through lane with a maximum width of fourteen (14) feet may be approved via a modification to a Special Use Permit. As shown on the proposed plans, three (3) lanes are proposed with a combined width of thirty-six and one-half (36.5) feet. According to the petitioner, the drive-through lanes will provide additional vehicle stacking on site to assist with site traffic flow and to prevent stacking onto Ziegler Drive and Wolf Road.

3. A modification to Section 6-306.B to increase the number of parking spaces from two (2) spaces to twenty-two (22) spaces (Section 6-306.B)

The Land Development Code requires one (1) parking space per employee for car wash uses. For this project, two (2) parking spaces are required to serve the two (2) employees. A total of twenty-two (22) parking spaces are proposed, which includes two (2) standard parking spaces, three (3) parallel parking spaces, fifteen (15) vacuum spaces, one (1) standard accessible space, and one (1) accessible vacuum space. Any request exceeding a twenty percent (20%) increase in the total number of parking spaces permitted on the lot must be approved through a modification to the Special Use Permit. The request for

additional parking will accommodate the vacuum canopies, which are an integral to the operations of car wash facilities, but are not adequately addressed by the Land Development Code.

Noise Control Requirements

The proposed car wash, including all vacuum equipment, must abide by all Noise Control Regulations set forth in the Title 6, Chapter 4 of the Village Code.

Signage

Two (2) monument signs are proposed, one along Wolf Road and the other along 159th Street. The conceptual location of the monument signs are shown on the submitted site plan. The conceptual appearance of the wall signs are indicated on the submitted building elevations and renderings. Signage is not included as part of this petition. All signs are subject to a separate sign permit review by the Development Services Department and must comply with Section 6-307 of the Land Development Code.

Accessory Structures

Low Wall - The approval for the Planned Development for Wolf Point Plaza by Ordinance No. 4558 included pedestrian amenities and site features such as a masonry low wall with a trellis, outdoor seating area, and unified monument signage. These site features were intended to provide visual interest, a harmonious design throughout the development, and a gateway feature at a prominent, highly-visible intersection in the Village. They were also intended to offset the proposed modifications/variances for the entire development as well as for the specific site plan for CVS proposed on the subject lot at that time, which included the allowance of a parking lot and one drive-through lane between the building and the street.

To meet the intent of the original approval and offset the current requested modification to allow for multiple drive-through lanes between the building and the street, the petitioner is proposing to install a masonry low wall surrounded by a mix of landscaping at the southwest corner of the site. The proposed low wall will primarily be constructed of the same gray brick used on the building with accent rows of the dark gray 4 inch spit face concrete masonry. The overall curved length of the wall is approximately 65 feet long. The central portion of the wall will measure just over 4 feet in height with taller brick columns topped with a decorative concrete cap on each end measuring 6 feet 11 inches in height.

Garbage Enclosure - A garbage enclosure is proposed on the east side of the site near the north cross access drives. As shown on the elevations, Sheet A305, the enclosure will be constructed of eight (8) foot tall brick walls that will match the brick used on the building and six (6) foot tall wood gates. As proposed, the garbage enclosure meets the requirements of the Land Development Code.

Vacuum Canopies - A total of sixteen (16) vacuum station canopies with an overall height of 10 feet 6 inches will be located in the parking lot to the east of the building. The curved canopies will be covered in a red fabric and extend over the parking spaces. Each vacuum canopy includes two vacuum hoses that hang approximately eleven (11) feet apart to allow for customers to vacuum both sides of their vehicles. Red accent balls will be located between the vacuum canopy posts on the adjacent concrete sidewalk to provide a holder for the vacuum hoses.

Payment Canopy - A payment canopy is proposed over the three drive-through lanes to the west of the building. The canopy measures approximately 35 feet long and 25 feet wide, with an overall height of 13 feet 7 inches. As shown on the submitted elevations, the black metal canopy will supported by four metal posts that will be wrapped with a four (4) foot tall brick base to match the brick building material and color. The wrapped brick base will help integrate the drive-through canopy with the building, offset the proposed modification to locate the drive-through lanes between the building and the street, and meet the code requirements per Section 6-207.F.4.

Mechanicals/Utility Conduits

All mechanical equipment must be screened, either at grade level with landscaping or hidden behind the roofline. Exterior roof ladders are not permitted and must be located interior to the building. The petitioner has verified that all mechanical equipment will be screened from view. The proposed roof ladder will be

accessed from an equipment room inside the building.

Exactions and/or Incentives

All exaction fees, including but not limited to transportation, parks and schools, are to be paid to the Village per Code requirements.

Bulk Requirements

Lot Size

Minimum - 10,000 square feet

Proposed - 66,932 square feet (1.54 acres)

Lot Coverage

Maximum - 75%

Proposed - 58.3%

Floor Area Ratio (FAR)

Maximum - 1.0

Proposed - 0.08

Setbacks

Street Setback (159th Street):

Required - 25 feet

Proposed - 43 feet

Street Setback (Wolf Road):

Required - 25 feet

Proposed - 107 feet to building; 70 feet to payment canopy

Side Setback (East Interior Setback):

Required - 15 feet

Proposed - 110 feet

Rear Setback (North - Ziegler Drive):

Required - 30 feet

Proposed - 53 feet

The petitioner has requested a modification to Section 6-207.F.4 to locate drive-through lanes in the setback areas between the building and the streets.

Building Height

Maximum - 4 stories or 50 feet, whichever is higher

Proposed - 28 feet to the top of the parapet wall; 20 feet 9 inches to the top of the fascia for the curved acrylic roof system

Parking and Loading

Required Parking Spaces - 2 parking spaces

Proposed Parking Spaces - 22 parking spaces

As noted above, the petitioner has requested a modification to Section 6-306.B to allow for an increase to the number of parking spaces from two (2) to twenty-two (22) spaces.

Required Stacking Spaces - 9 spaces (3 spaces per drive-through lane)

Proposed Stacking Spaces - 24 spaces from payment canopy to end of drive-through lanes; 29 spaces between car wash building entrance to end of drive-through lanes (5 stacking spaces between payment canopy and car wash entrance)

This case is now before Plan Commission for consideration.

BUDGET IMPACT:

REQUESTED ACTION:

Regarding Case Number **2018-0619**, also known as **Tommy's Car Wash**, I move to accept and make findings of fact as discussed at this Plan Commission meeting and within the staff report dated April 16, 2019.

I move to recommend to the Village Board approval of the **Preliminary Site Plan** titled "Preliminary Site Plan", prepared by V3 Companies, Sheet C1.0, dated August 31, 2018 and last revised April 1, 2019, subject to the following conditions:

1. Meet all building code requirements and final engineering requirements, including required permits from outside agencies.
2. Screen all mechanical equipment either at grade or at rooftop with landscaping or parapets respectively.
3. Submit a sign permit application to the Development Services Department for separate review. Signs are subject to additional review and approval via the sign permitting process and additional restrictions may apply.
4. Provide a cross access easement from Ziegler Drive to the cross access drives on the east property line.
5. The photometric plan shall comply with all lighting requirements per Section 6-315.

And

I move to recommend to the Village Board approval of the **Elevations** titled "Exterior Elevations", Sheet A201, dated July 27, 2018, and "Building and Wall Sections", Sheet A305, dated September 20, 2018, prepared by Progressive Architecture Engineering, PC, subject to the following conditions:

1. Screen all mechanical equipment either at grade level with landscaping or hidden behind the roofline.
2. All masonry must be of anchored veneer type masonry with a 2.625" minimum thickness.
3. Submit a sign permit application to the Development Services Department for separate review. Signs are subject to additional review and approval via the sign permitting process and additional restrictions may apply.
4. Meet all final engineering and building code requirements.

And

I move to recommend to the Village Board of Trustees approval of the **Preliminary Landscape Plan**, titled "Landscape Plan", Sheet L.1-0, L-2.0, L-3.0, dated February 12, 2019, and "Wall Elevation", Sheet LSK-1, dated February 8, 2019, prepared by Daniel Weinbach & Partners, Ltd., with the following conditions:

1. Submit a final landscape plan meeting all Village Codes and all required supporting documentation addressing all outstanding landscape items in conjunction with the final engineering submittal.

And

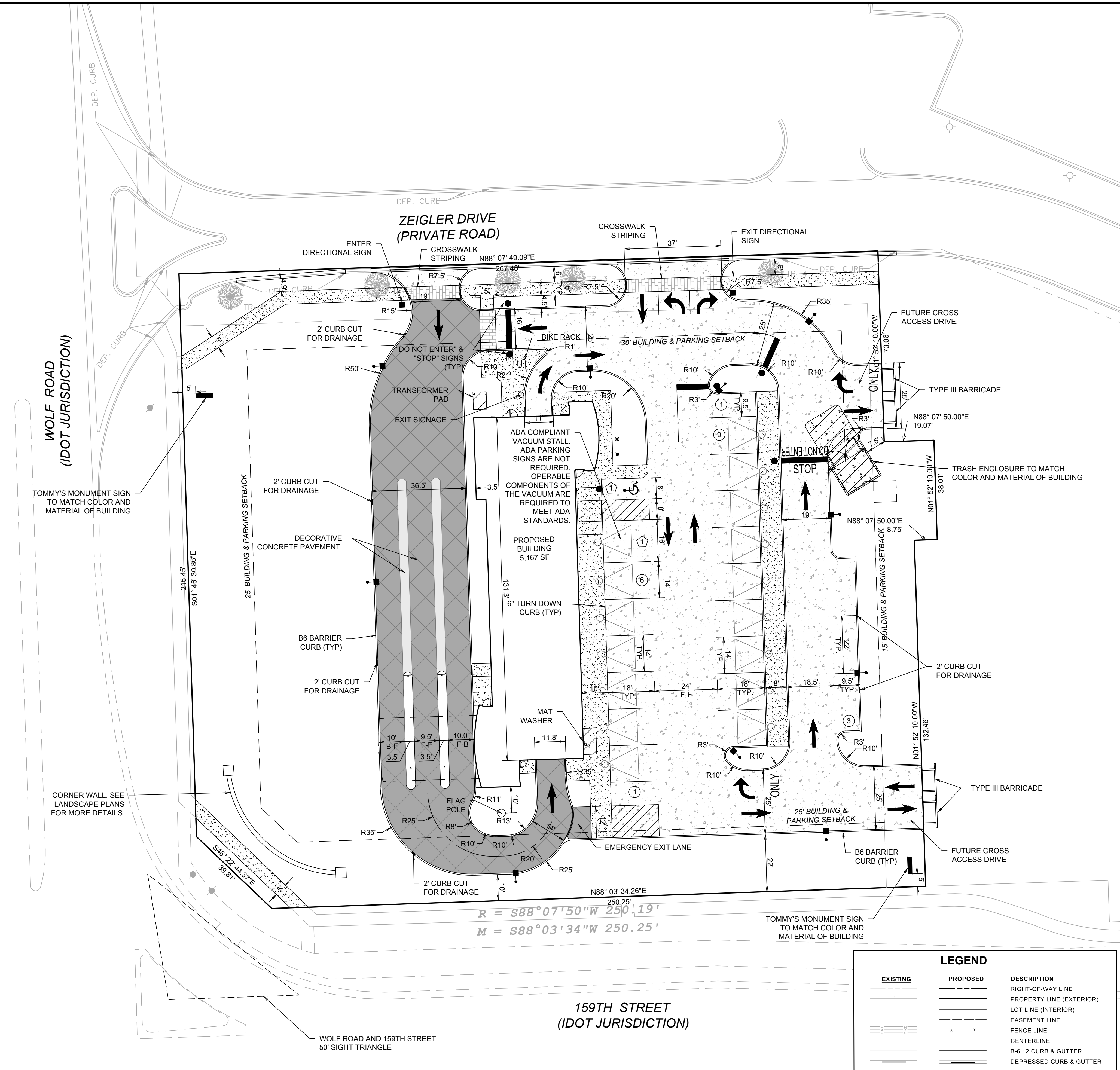
I move to recommend to the Village Board approval of a **Special Use Permit Amendment** for Wolf Point

Plaza Planned Development to allow for the construction and operation of Tommy's Car Wash, a car wash in the BIZ General Business District, subject to the same conditions as outlined in the Preliminary Site Plan motion and the following conditions:

1. Meet all Noise Control Regulations set forth in the Title 6, Chapter 4 of the Village Code.

Modifications to the Special Use Permit include:

1. A modification to locate drive-through lanes in the setback areas between the building façade and the street (Section 6-207.F.4)
2. A modification to increase the number of drive-through lanes between the building façade and the street from one (1) lane with a maximum width of fourteen (14) feet to three (3) lanes with a combined width of thirty-six and one-half (36.5) feet (Section 6-207.F.4)
3. A modification to Section 6-306.B to increase the number of parking spaces from two (2) spaces to twenty-two (22) spaces (Section 6-306.B)



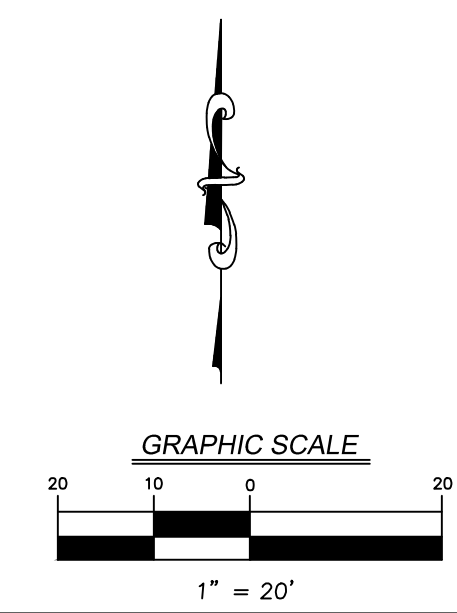
- NOTES:**
- ALL DIMENSIONS SHOWN ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
 - ALL PROPOSED ON-SITE STRIPING SHALL BE PAINTED YELLOW UNLESS OTHERWISE NOTED.
 - BUILDING DIMENSIONS ARE TO OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - ALL CURB AND GUTTER SHALL BE B6.12 UNLESS OTHERWISE NOTED.

SITE SUMMARY	
LOT AREA (1)	GROSS & NET AREA: 66,932 SF. (1.54 AC.)
PROPOSED BUILDING (1)	5,167 SF.
LANDSCAPED AREA	27,892 SF. (0.64 AC.): 42% SITE COVERAGE
GROSS IMPERVIOUS SURFACE AREA	39,040 SF. (0.90 AC.): 58% SITE COVERAGE
FLOOR AREA RATIO	0.08

PARKING SUMMARY	
REQUIRED STALLS	
DRIVE-THROUGH: 3 STACKING SPACES PER LANE OPERATIONS: 1 STALL PER EMPLOYEE (2 EMPLOYEES AT PEAK SHIFT)	= 3
BICYCLE: 3 WITHIN A BICYCLE RACK	= 3
PROVIDED STALLS	
STANDARD 9' STALLS	= 2
PARALLEL PARKING STALLS (FOR EMPLOYEES)	= 3
VACUUM 14' STALLS	= 15
ACCESSIBLE 16' STALLS	= 1
VACUUM ACCESSIBLE 16' STALLS	= 1
TOTAL STALLS PROVIDED	= 22
BICYCLE STALLS IN RACK	= 5

PAVING LEGEND	
NOTE - PAVEMENT SECTIONS TO BE CONFIRMED BY GEOTECHNICAL REPORT WHEN AVAILABLE	
CONCRETE PAVEMENT	
	6" P.C. CONCRETE PAVEMENT WITH 6"x6" NO. 10 WWF 4" AGGREGATE BASE COURSE - CA6
CONCRETE PAVEMENT FOR TRASH ENCLOSURE	
	8" P.C. CONCRETE PAVEMENT WITH 6"x6" NO. 10 WWF 4" AGGREGATE BASE COURSE - CA6
STAMPED CONCRETE	
	6" P.C. CONCRETE PAVEMENT WITH 6"x6" NO. 10 WWF 4" AGGREGATE BASE COURSE - CA6
COLORED CONCRETE	
	6" P.C. CONCRETE PAVEMENT WITH 6"x6" NO. 10 WWF 4" AGGREGATE BASE COURSE - CA6
CONCRETE SIDEWALK	
	5" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE COURSE - CA6

EXISTING	PROPOSED	DESCRIPTION
		RIGHT-OF-WAY LINE
		PROPERTY LINE (EXTERIOR)
		LOT LINE (INTERIOR)
		EASEMENT LINE
		FENCE LINE
		CENTERLINE
		B-6.12 CURB & GUTTER
		DEPRESSED CURB & GUTTER
		REVERSED PITCH B-6.12 CONCRETE CURB AND GUTTER



REVISIONS		
NO.	DATE	DESCRIPTION
1	11-30-18	VILLAGE COMMENTS
2	02-28-19	VILLAGE COMMENTS
3	04-01-19	VILLAGE COMMENTS

PROJECT NO.: 18257
 ORIGINAL ISSUE DATE: 8-31-18
 PROJECT MANAGER: AU
 DESIGNED BY: DP
 DRAWN BY: DB

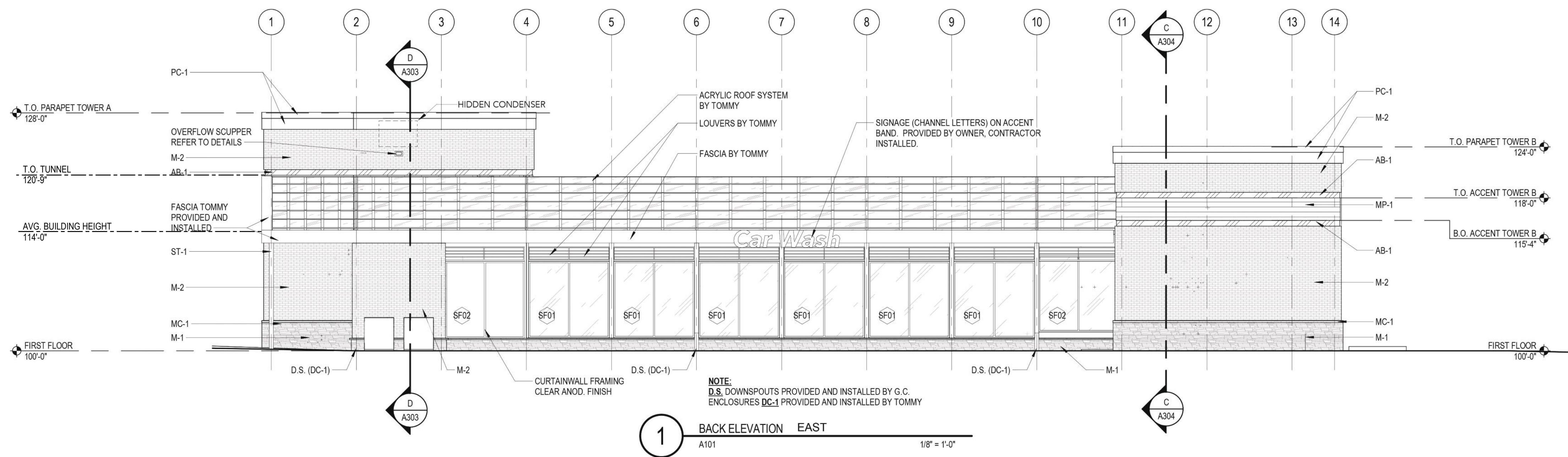
PRELIMINARY SITE PLAN

TOMMY'S CAR WASH

ORLAND PARK ILLINOIS

7325 James Avenue
 Woodridge, IL 60517
 630.724.9200 phone
 www.v3co.com

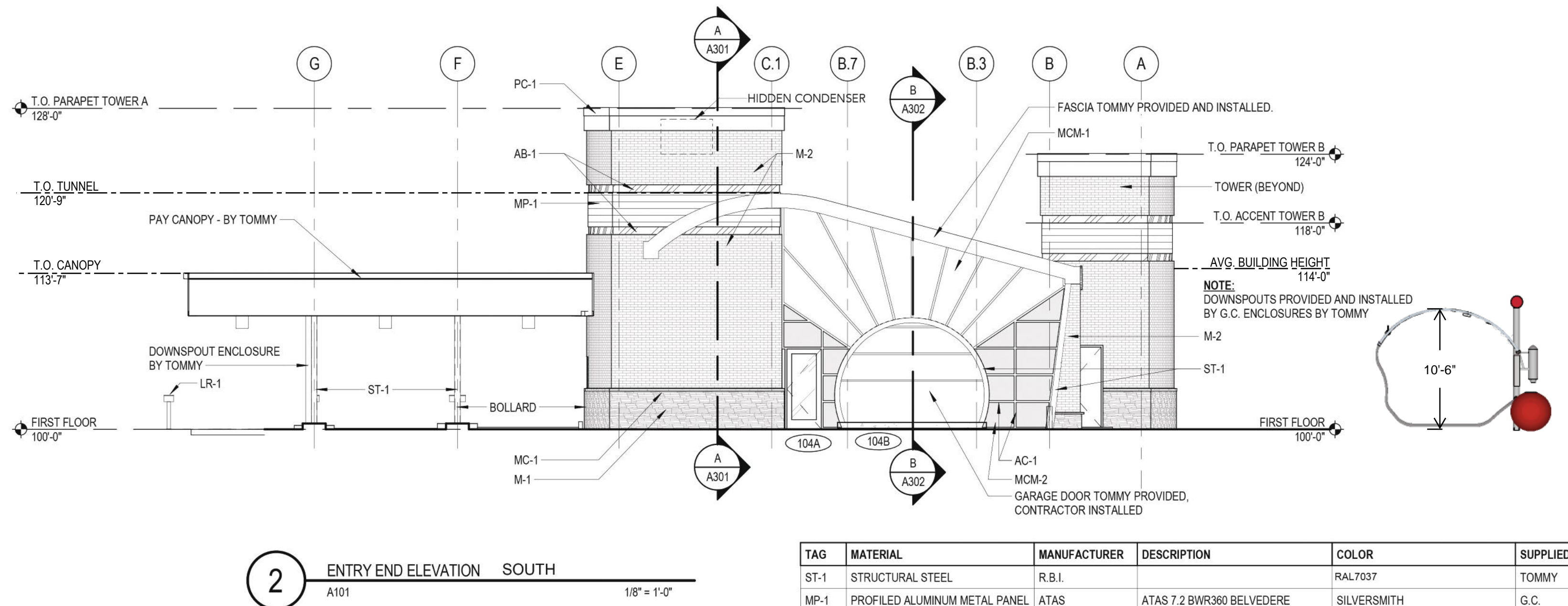
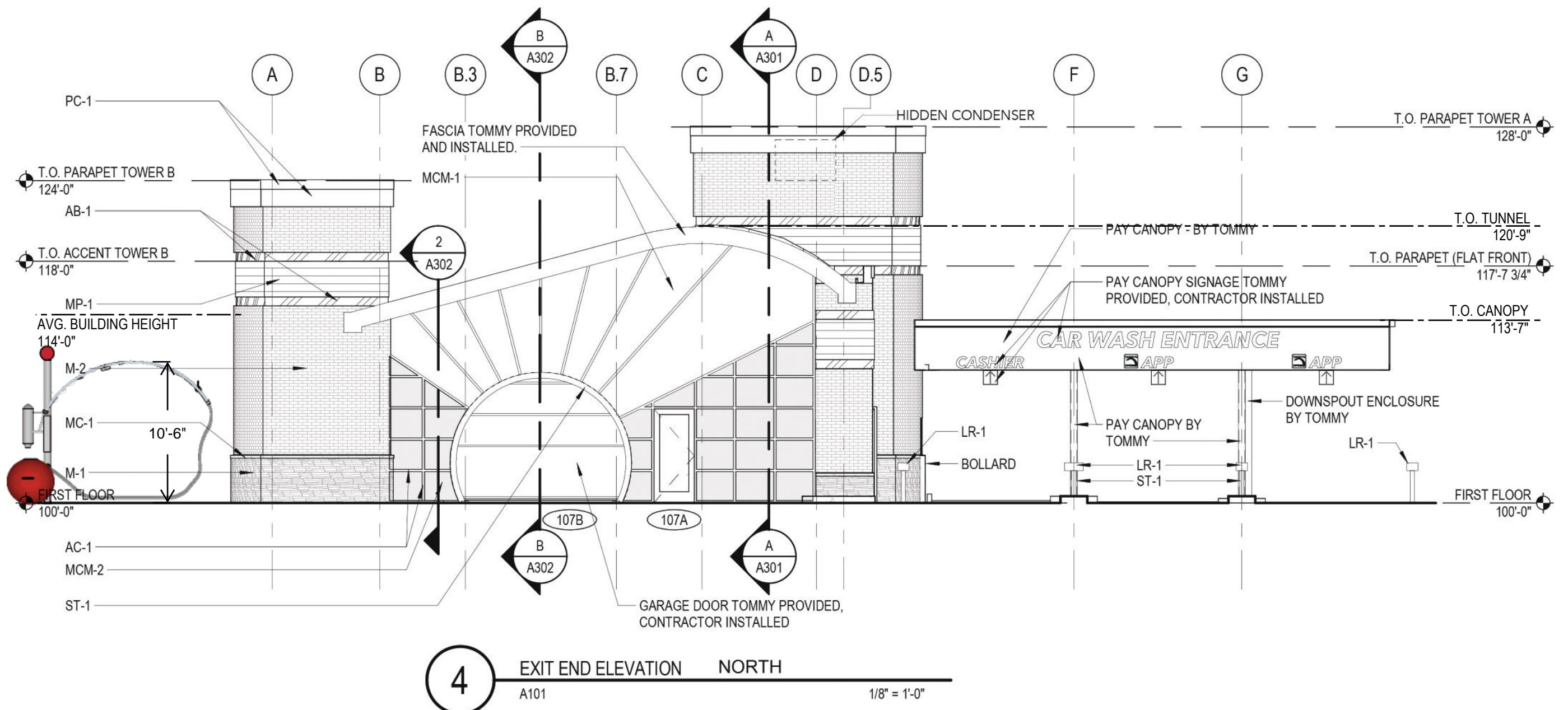
DRAWING NO.
C1.0



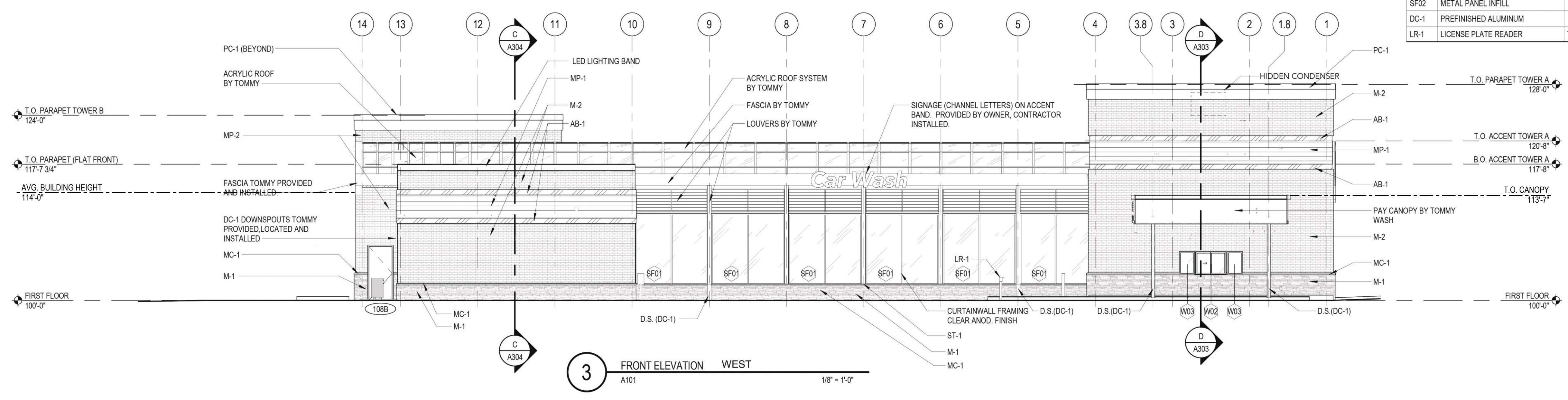
EXTERIOR MATERIALS LEGEND

- PREFINISHED METAL COPING
- ALUM. FLAT STOCK (BLACK)
- CORRUGATED METAL PANEL
- MCM PANEL (BLACK)
- GLAZING
- BRICK VENEER
- SPLIT FACED CONCRETE MASONRY UNIT

NOTE: ALL CORRUGATED METAL PANEL ACCENT (MP-1) SHALL BE CRIMP CURVED AT CURVED FACE OF TOWERS AND SHALL HAVE MITERED CORNERS. NO CORNER TRIMS. NO EXCEPTIONS



TAG	MATERIAL	MANUFACTURER	DESCRIPTION	COLOR	SUPPLIED BY
ST-1	STRUCTURAL STEEL	R.B.I.		RAL7037	TOMMY
MP-1	PROFILED ALUMINUM METAL PANEL	ATAS	ATAS 7.2 BWR360 BELVEDERE	SILVERSMITH	G.C.
AB-1	PREFIN. ALUM. ACCENT BANDING	ATAS	ATAS FLAT SHEET	BLACK	G.C.
PC-1	PREFIN. ALUM. PARAPET CORNICE	REYNOLUX		BRIGHT SILVER METALLIC	G.C.
M-1	SPLIT-FACE CMU	CONSUMERS	4" VENEER SPLIT-FACED	ASH	G.C.
M-2	BRICK	GLEN-GERY	KLAYCOAT BRICK	STONE GRAY	G.C.
MC-1	PRECAST STONE		MASONRY CAP	MATCH CMU	G.C.
MCM-1	PREFIN. ALUM. COMPOSITE PANELS	LAMINATORS	LAMINATORS OMEGA SERIES	SUNSET RED	G.C.
MCM-2	PREFIN. ALUM. COMPOSITE PANELS	ALPOLIC	CITADEL GLAZE GUARD	EBONY	G.C.
AC-1	PREFIN. ALUM. CLOSURE CAPS	TUBELITE	TUBELITE 200 SERIES CURTAINWALL	CLEAR ANNOXIDIZED	G.C.
SF01	INSULATED GLASS		1" INSULATED GLASS LOW-E	CLEAR	G.C.
SF02	METAL PANEL INFILL		1" SPANDREL PANEL	BLACK	G.C.
DC-1	PREFINISHED ALUMINUM		DOWNSPOUT COVER	RAL7037	TOMMY
LR-1	LICENSE PLATE READER	TOMMY	BOLLARD	STAINLESS STEEL	TOMMY

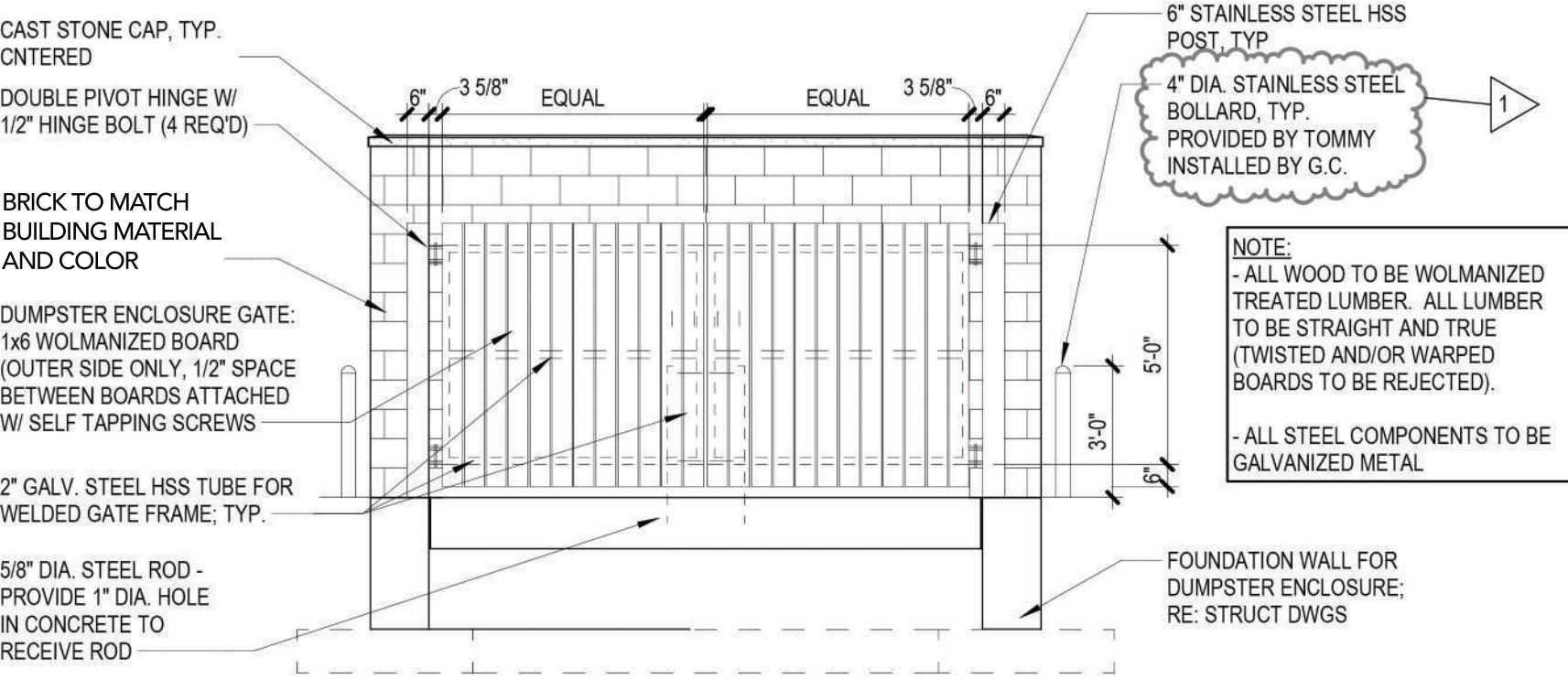


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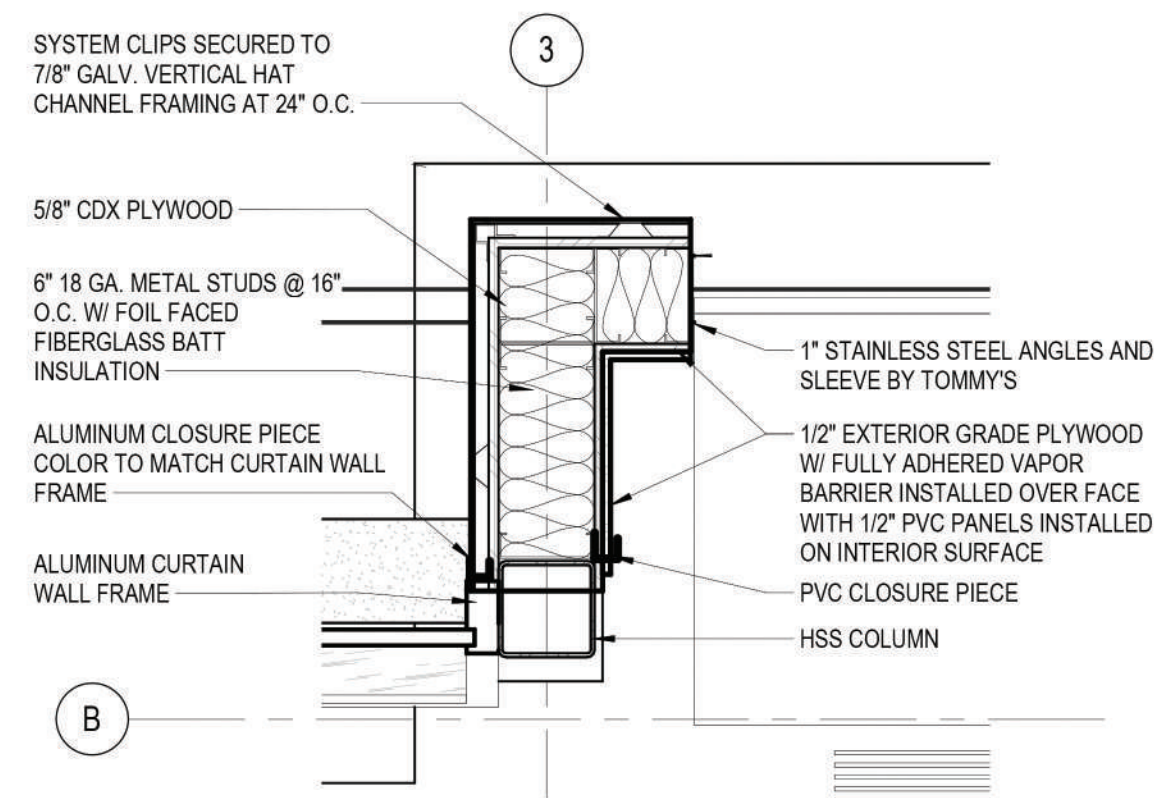
ISSUANCES
 Permits and Construction
 07/27/2018

REVISIONS
 NO. DATE DESCRIPTION

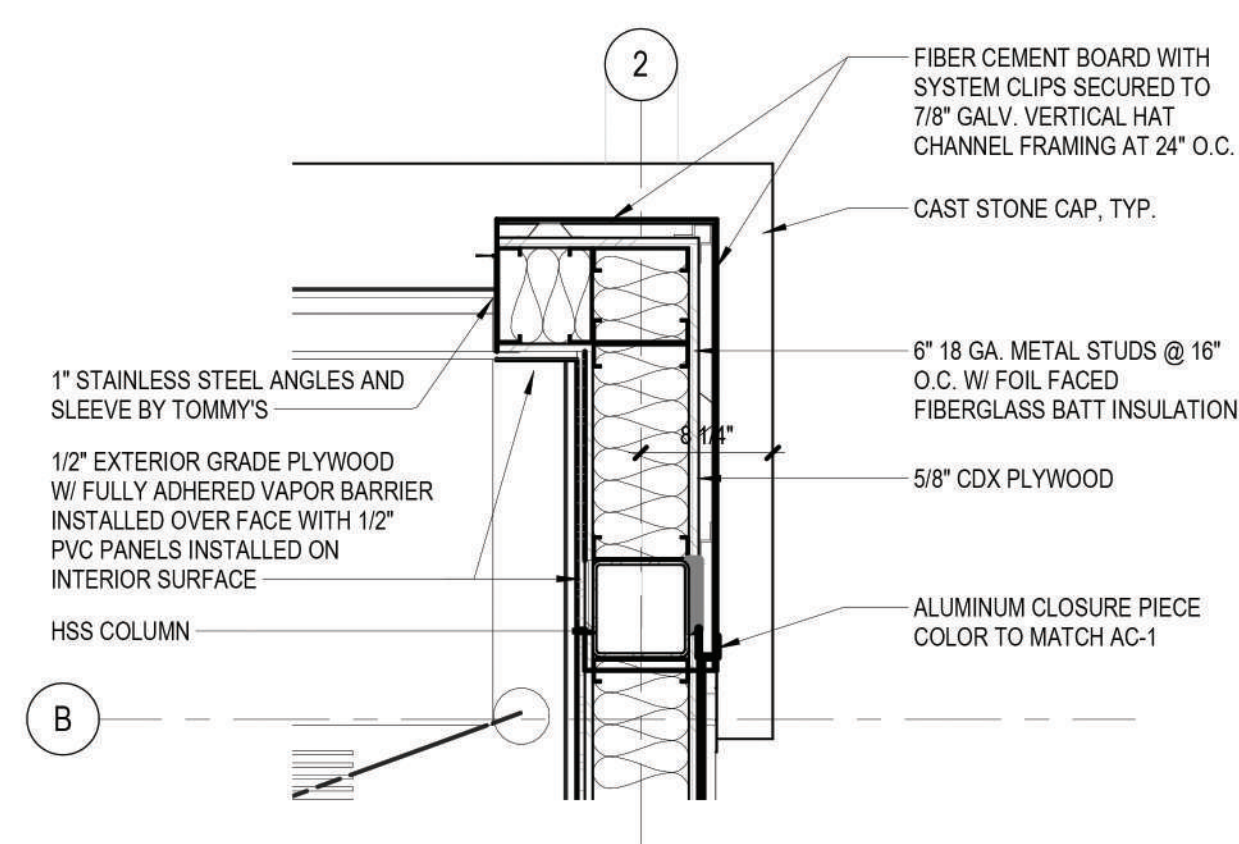
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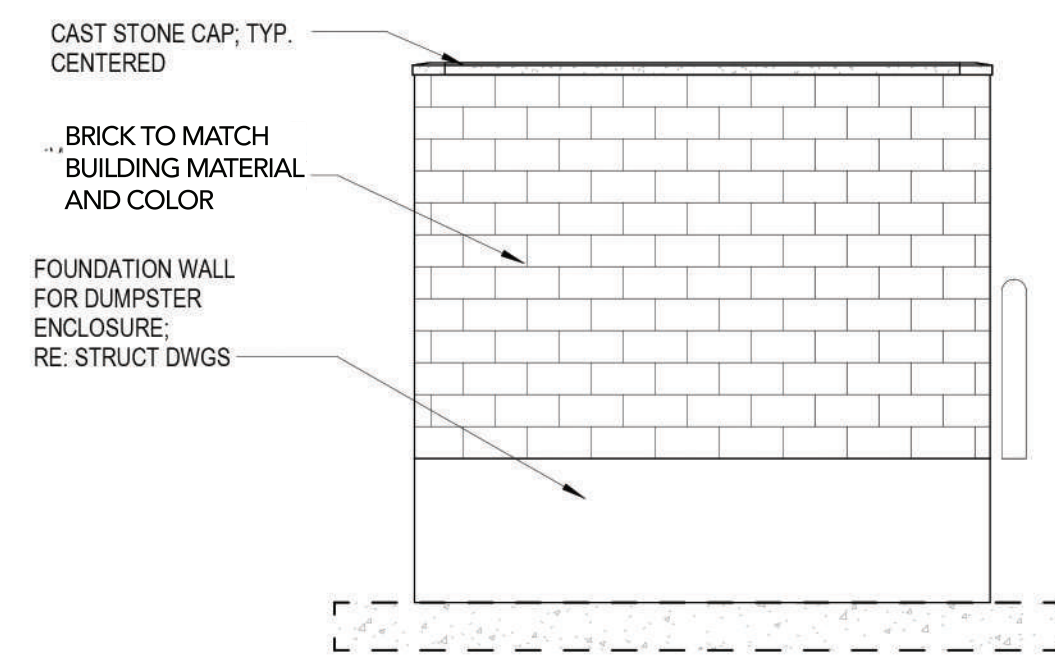
5 DUMPSTER ENCLOSURE - FRONT ELEVATION
A305 1/4" = 1'-0"



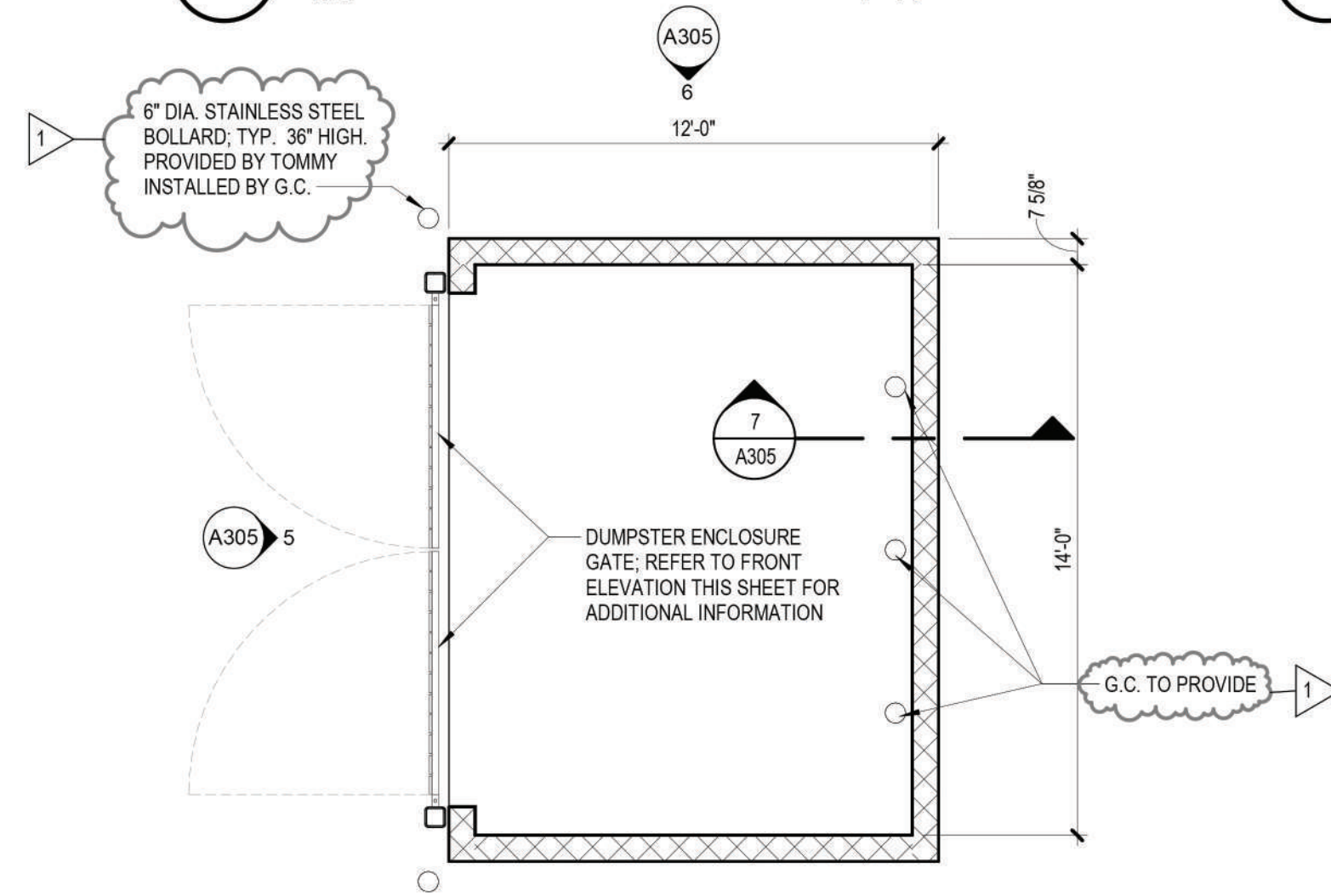
3 DETAIL - MAT WASHER BUMPOUT EAST
A101 1" = 1'-0"



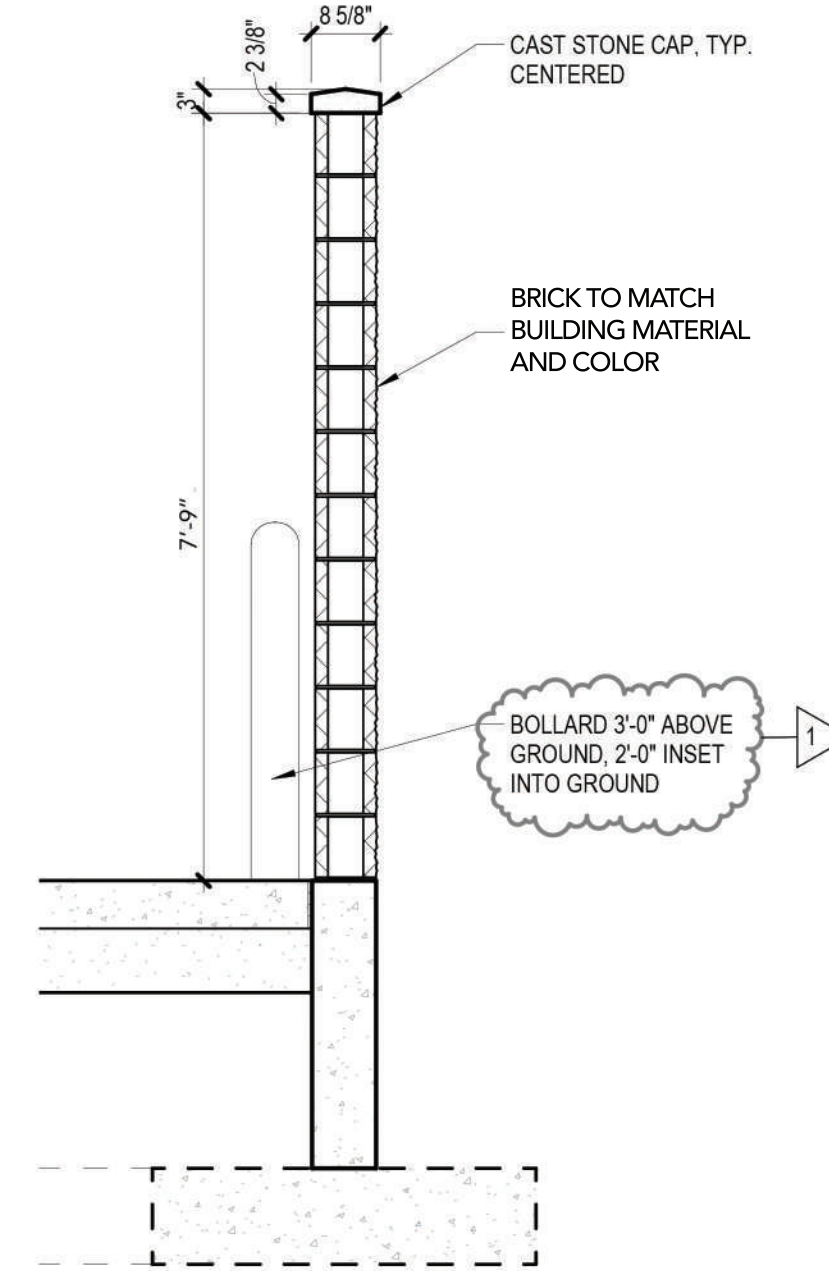
2 DETAIL - MAT WASHER BUMPOUT WEST
A101 1" = 1'-0"



6 DUMPSTER ENCLOSURE - SIDE ELEVATION
A305 1/4" = 1'-0"



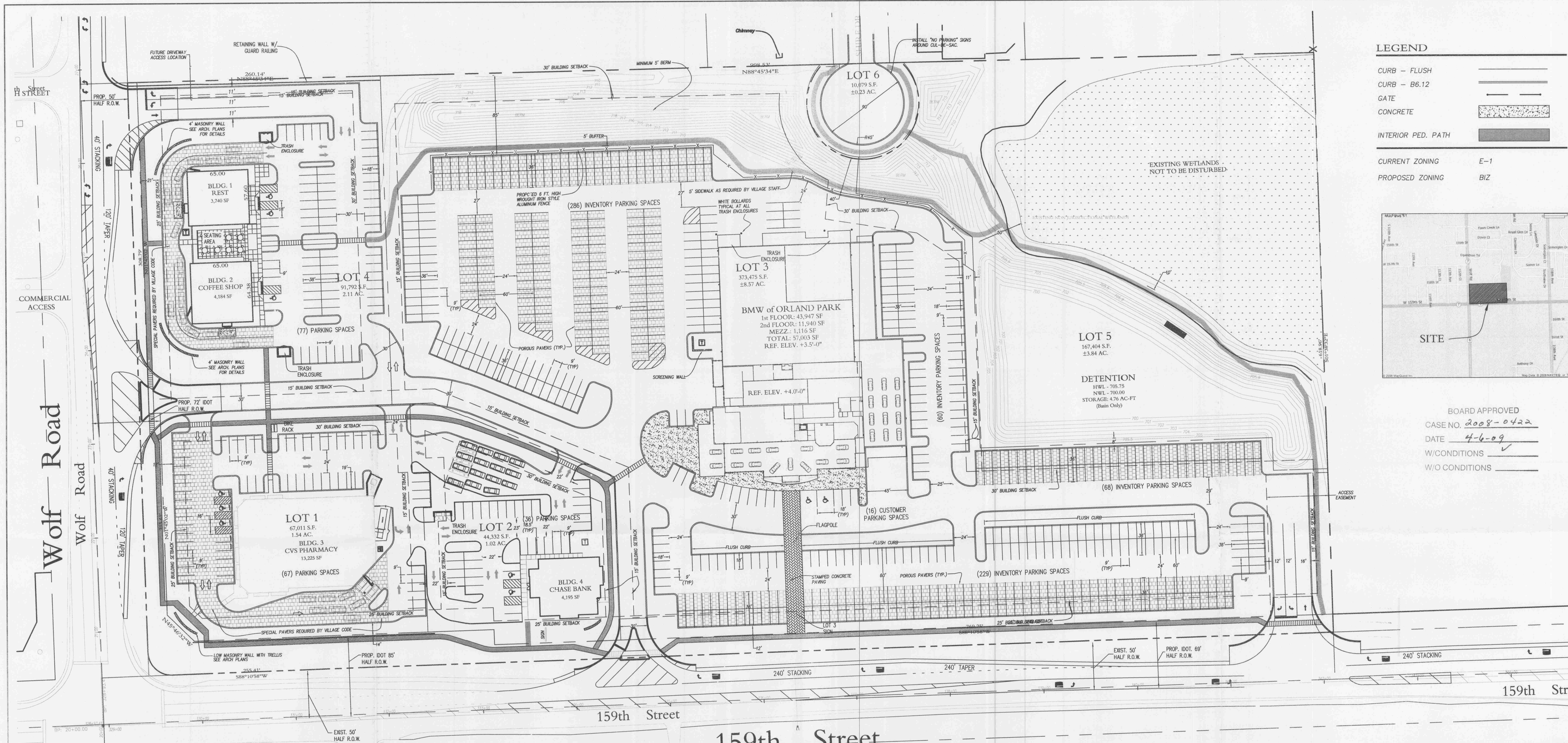
4 DUMPSTER ENCLOSURE PLAN
A201 1/4" = 1'-0"



7 DUMPSTER ENCLOSURE - WALL SECTION
A305 1/2" = 1'-0"

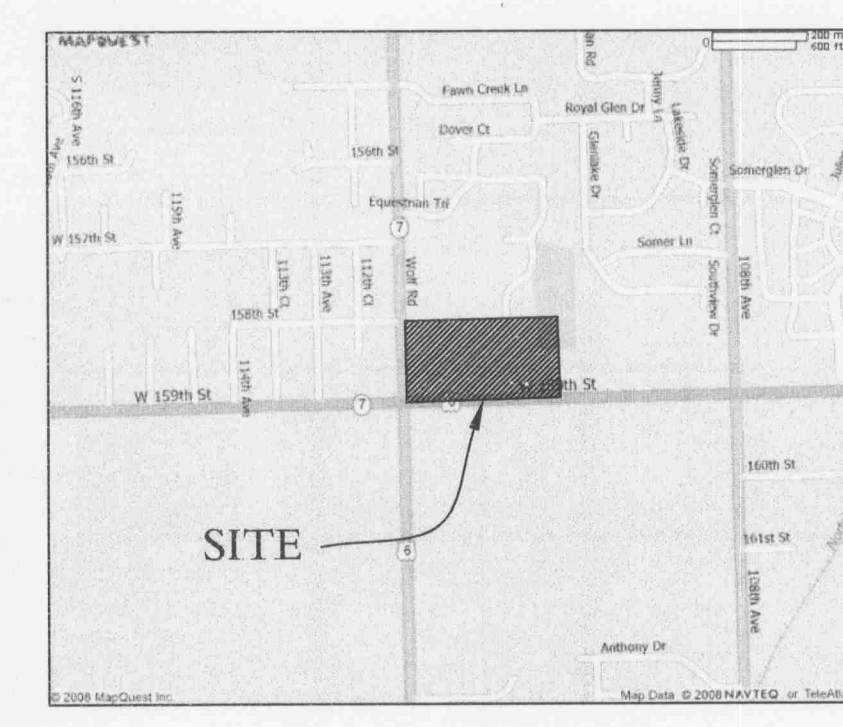
If this document is sealed and signed in a digital or electronic format and is received from someone other than the sealing professional identified in the document, you must contact the sealing professional in writing to validate authenticity of the document. The sealing professional disclaims the seal and signature and shall not be liable for any liability associated with it where the authenticity of any digital or electronic seal or signature has not been validated in this manner.

NO.	DATE	DESCRIPTION
1	09/20/18	REVISED 9/20/18



LEGEND

CURB - FLUSH	---
CURB - B6.12	---
GATE	---
CONCRETE	▒
INTERIOR PED. PATH	▒
CURRENT ZONING	E-1
PROPOSED ZONING	BIZ



BOARD APPROVED
 CASE NO. 2008-0422
 DATE 4-6-09
 W/CONDITIONS ✓
 W/O CONDITIONS

SITE

LOT 1	±69,329 SF	±1.59 ACRES
LOT 2	±41,895 SF	±0.96 ACRES
LOT 3	±376,250 SF	±8.64 ACRES
LOT 4	±91,468 SF	±2.10 ACRES
LOT 5	±164,695 SF	±3.78 ACRES
LOT 6	±10,079 SF	±0.23 ACRES
ROW TO BE DEDICATED	±39,172 SF	±0.90 ACRES
EXISTING WETLAND AREA (Part of Lot 5)	±71,774 SF	±1.65 ACRES
TOTAL	±793,267 SF	±18.21 ACRES

LAND USE

TOTAL AREA (W/O ROW & WETLANDS)	±682,321 SF	±15.66 ACRES
LANDSCAPING AREA	±189,739 SF	±4.36 ACRES
IMPERVIOUS AREA	±395,947 SF	±9.08 ACRES
PERMEABLE PAVERS AREA	±67,946 SF	±1.56 ACRES
BOTTOM OF BASIN AREA (100% IMPERVIOUS)	±24,273 SF	±0.56 ACRES
% TOTAL LANDSCAPE COVERAGE	27.84%	

PARKING

TOTAL BMW PARKING PROVIDED	659 CARS
INVENTORY	643 CARS
CUSTOMER	16 CARS
	11.56 CARS/1000 SF
TOTAL RETAIL PARKING PROVIDED	175 CARS
BUILDINGS 1 & 2 (RESTAURANT)	77 CARS
PROVIDED 9.71 CARS/1000 SF (GLA)	
REQ'D 10.00 CAR/1000 SF (GLA)	80 CARS
BUILDING 3 (CVS PHARMACY)	67 CARS
PROVIDED 4.07 CARS/1000 SF (GLA)	
REQUIRED 5.00 CARS/1000 SF	66 CARS
BUILDING 4 (CHASE BANK)	36 CARS
PROVIDED 8.58 CARS/1000 SF (GLA)	
REQUIRED 5.00 CARS/1000 SF	21 CARS

BUILDING COVERAGE

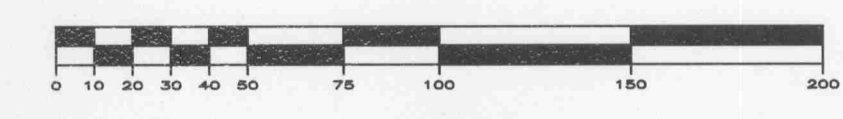
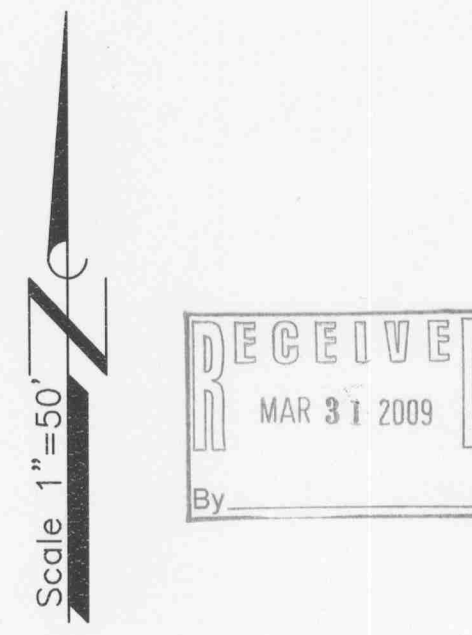
LOT 1	CVS PHARMACY (BUILDING 3)	13,225 SF	19.74%
BUILDING LOT COVERAGE			
LOT 2	CHASE BANK (BUILDING 4)	4,195 SF	9.46%
BUILDING LOT COVERAGE			
LOT 3	BMW DEALERSHIP	57,003 SF	15.15%
BUILDING LOT COVERAGE			
LOT 4	RESTAURANT (BUILDING 1)	3,740 SF	
RESTAURANT (BUILDING 2)	4,184 SF		
TOTAL BUILDING AREA	7,924 SF		
BUILDING LOT COVERAGE	8.66%		

MODIFICATIONS

- REDUCE THE WETLAND BUFFER FROM 50' TO 10'.
- REDUCE THE PARKING STALL LENGTH FROM 18.5' TO 18'.
- REDUCE THE DETENTION SETBACK FROM 25' TO 8'.
- INCREASE THE ALLOWABLE OUTDOOR STORAGE ON LOT 3.
- REDUCE THE LANDSCAPE BUFFER AND PARKING LOT ISLAND REQUIREMENTS AS SHOWN ON THE PRELIMINARY SITE PLAN.
- REDUCE THE HANDICAP PARKING REQUIREMENTS ON LOT 3 FROM 6 STALLS TO 2 STALLS.

VARIANCES

- ALLOW FOR PARKING IN THE STREET SETBACK ON LOTS 1 & 3.
- ALLOW FOR DRIVE-THRU LANES IN THE STREET SETBACKS ON LOTS 1 & 4.
- ALLOW FOR VEHICLE SERVICE REPAIR WITHIN 200' OF A RESIDENTIAL USE ON LOT 3.



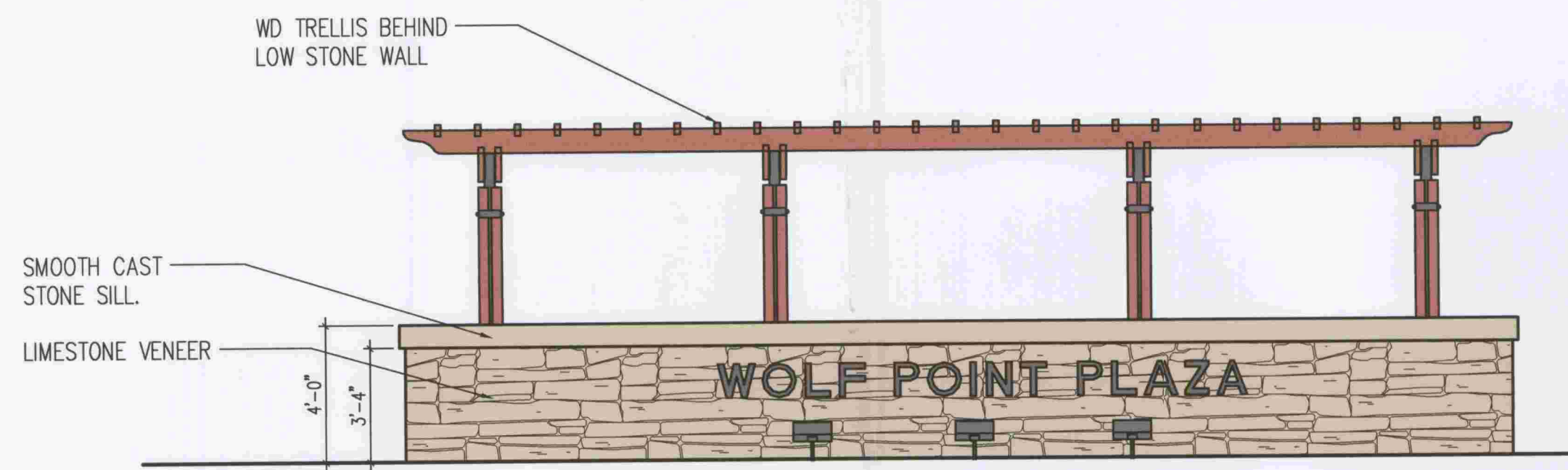
REVISIONS

NO.	DATE	DESCRIPTION

PRELIMINARY PLANNED UNIT DEVELOPMENT PLAN

WOLF POINT PLAZA
 NEC 159th STREET AND WOLF ROAD
 ORLAND PARK, ILLINOIS

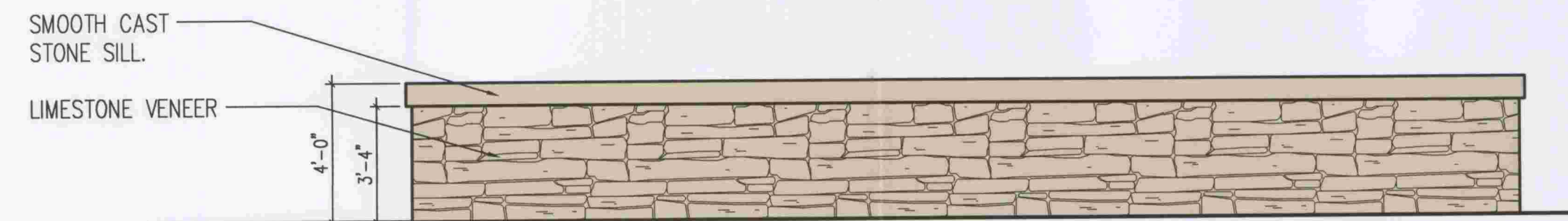
Craig R. Knoche & Associates • Civil Engineers
 • Surveyors
 • Land Planners
 24 N. Bennett Street • Geneva, IL 60134 • phone (630) 845-1270 • fax (630) 845-1275
 DATE: 2-02-09
 FILE: 8048-PUD
 JOB NO: 8-048
C1.3
 SHEET NO.



LOW WALL / TRELLIS

SCALE: 1/4" = 1'-0"

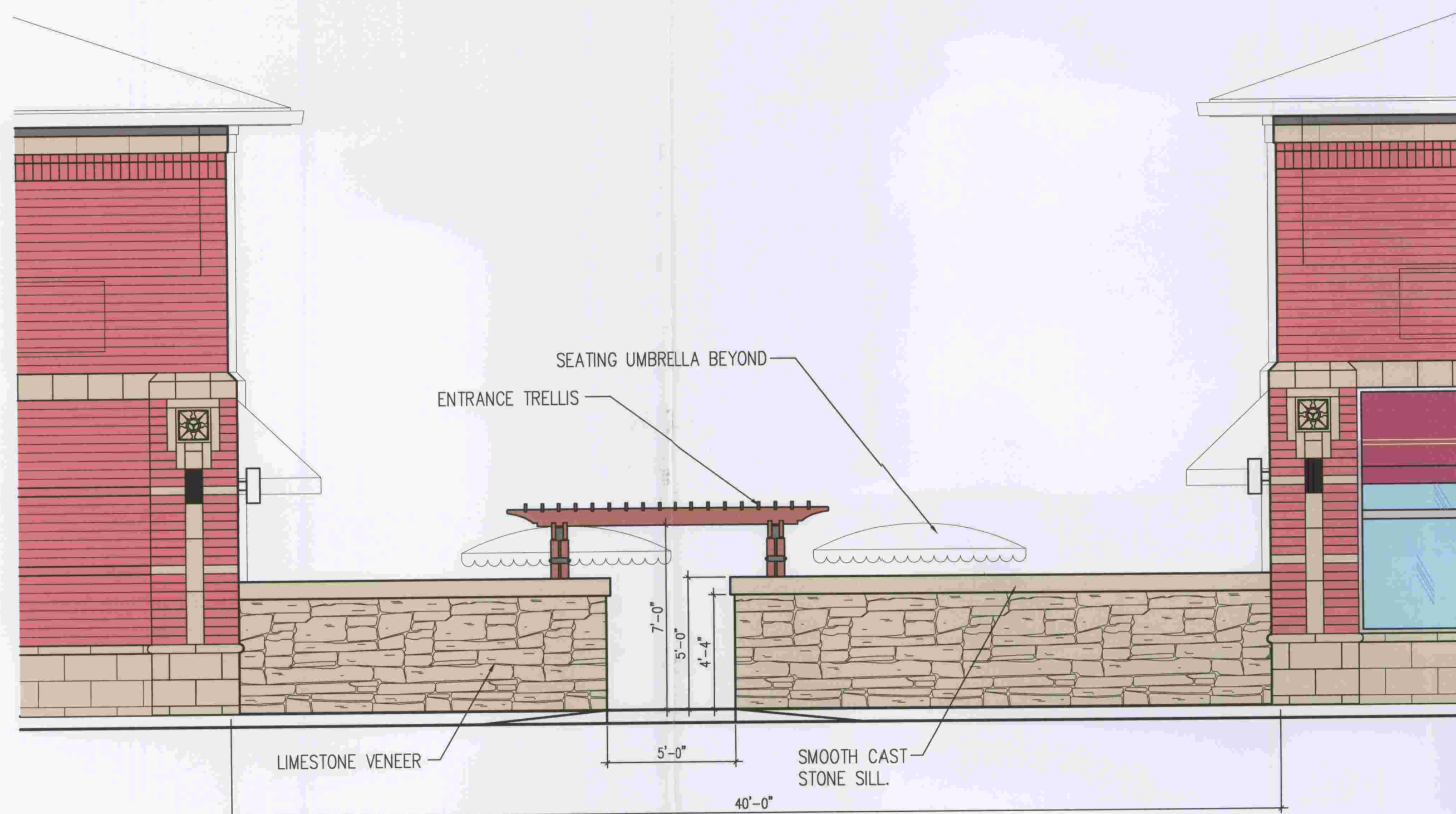
NOTE: ADJACENT TO CVS



LOW WALL

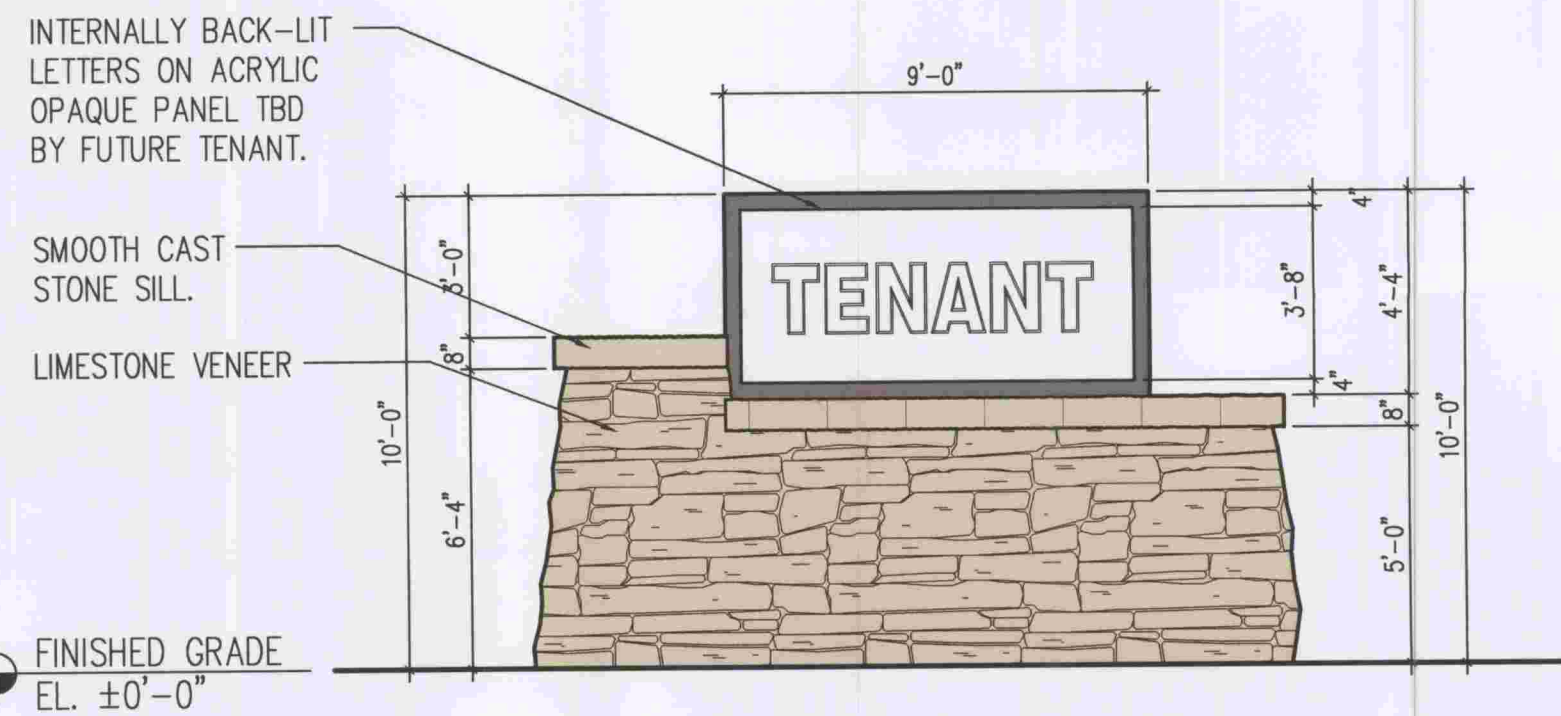
SCALE: 1/4" = 1'-0"

NOTE: ADJACENT TO RETAIL SHOPS AND CHASE BANK



SEATING AREA WALL

SCALE: 1/4" = 1'-0"

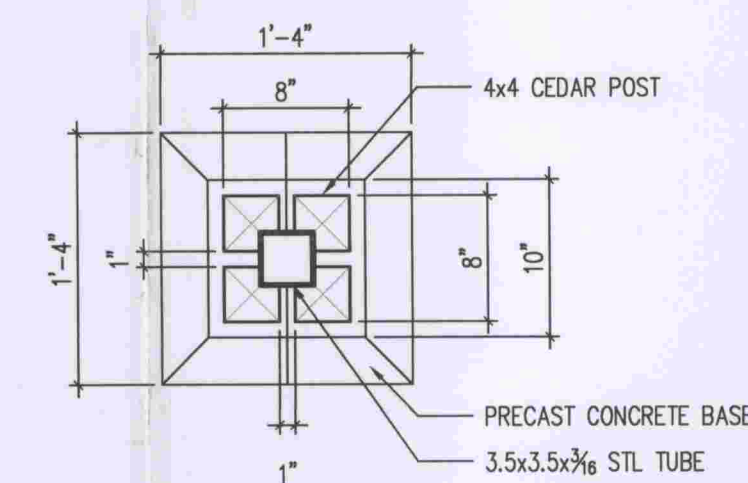
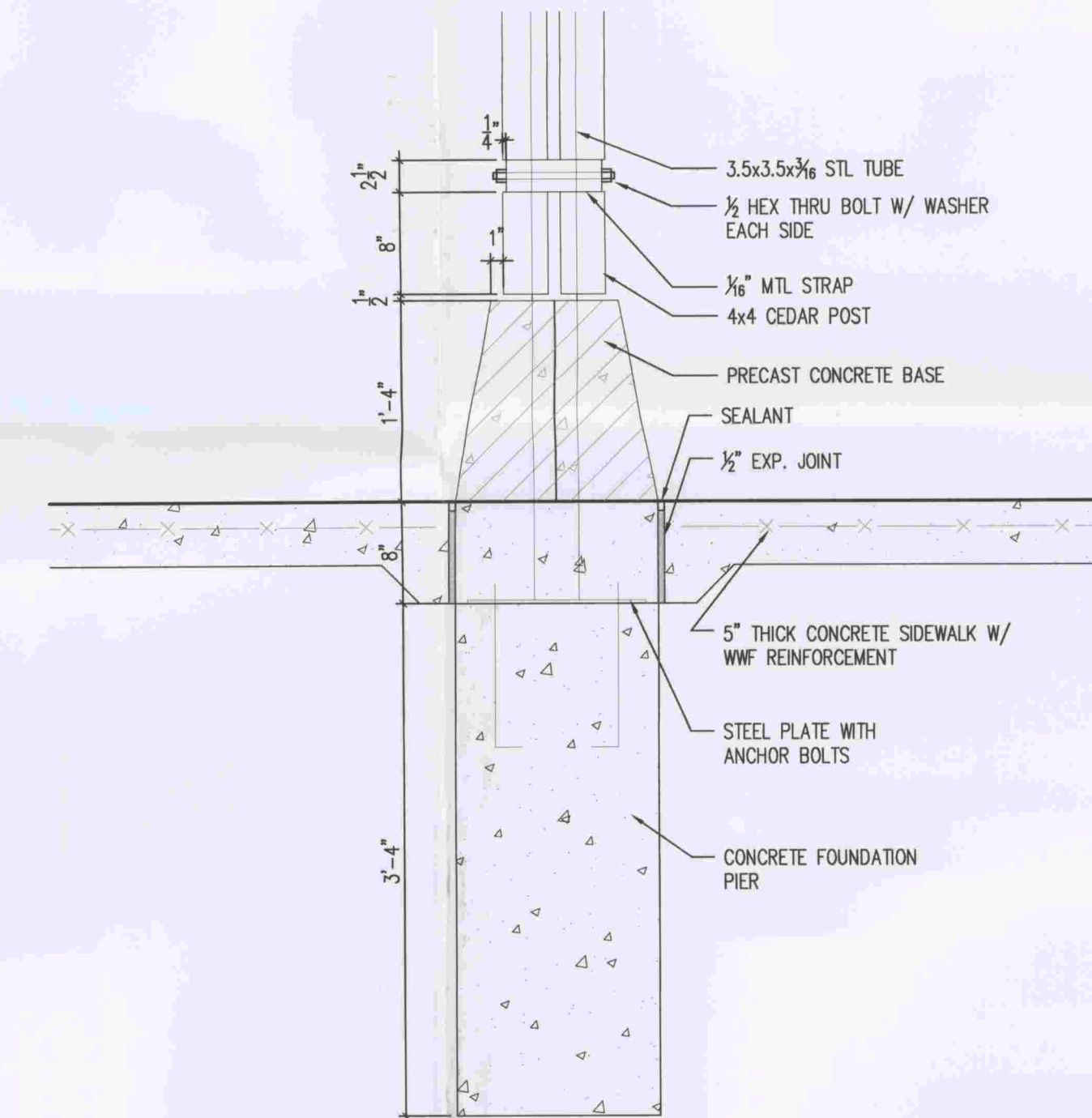
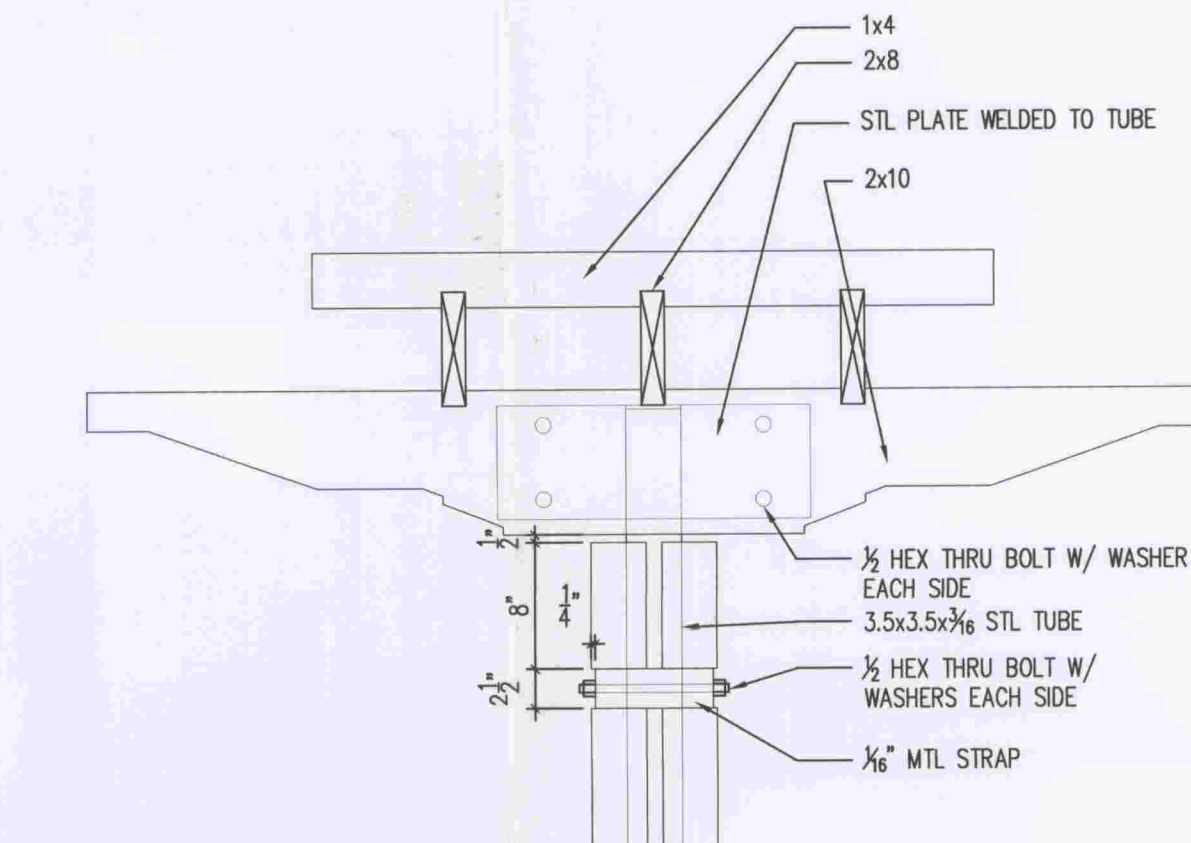


MONUMENT SIGN

SCALE: 1/4" = 1'-0"

FINISHED GRADE
EL. ±0'-0"

BOARD APPROVED
CASE NO. 2008-0422
DATE 4-6-09
W/CONDITIONS
W/O CONDITIONS



TRELLIS DETAILS

SCALE: 1" = 1'-0"

REVISIONS

2-17-09

DRAWN MDI

APPROVED BRW

ISSUED FOR REVIEW

DATE 1-16-09

FIELD BOOK

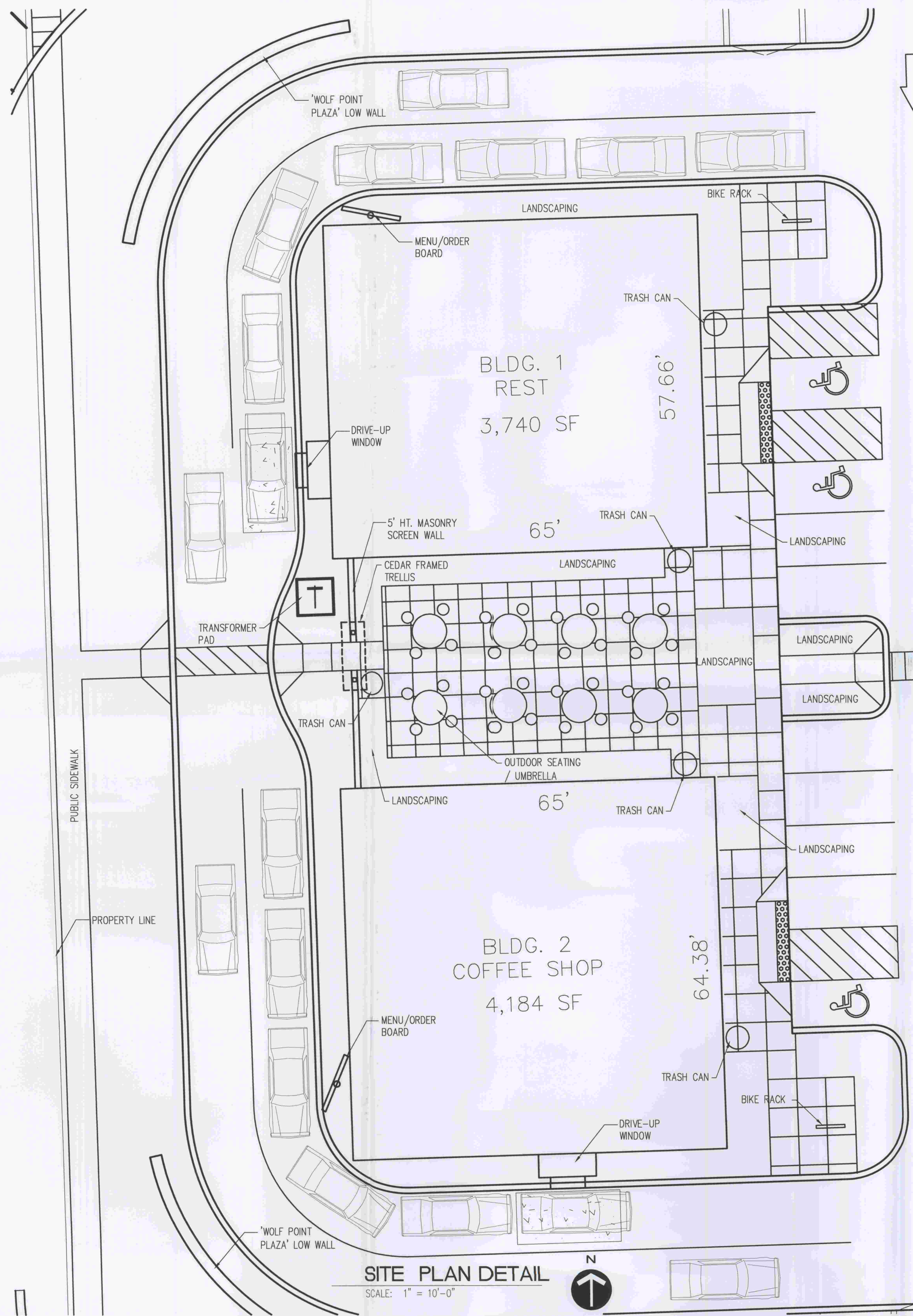
SHEET NAME

**MONUMENT SIGN/
LOW WALL/
AND PERGOLA
ELEVATIONS**

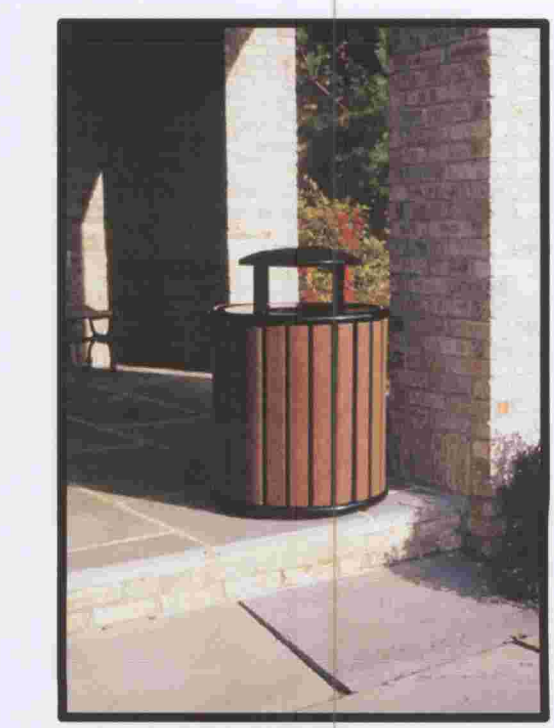
PROJECT NO. 8081570

SHEET NO.

A23



BIKE RACK - DERO ROLLING RACK
BY DERO BIKE RACKS
COLOR: BLACK

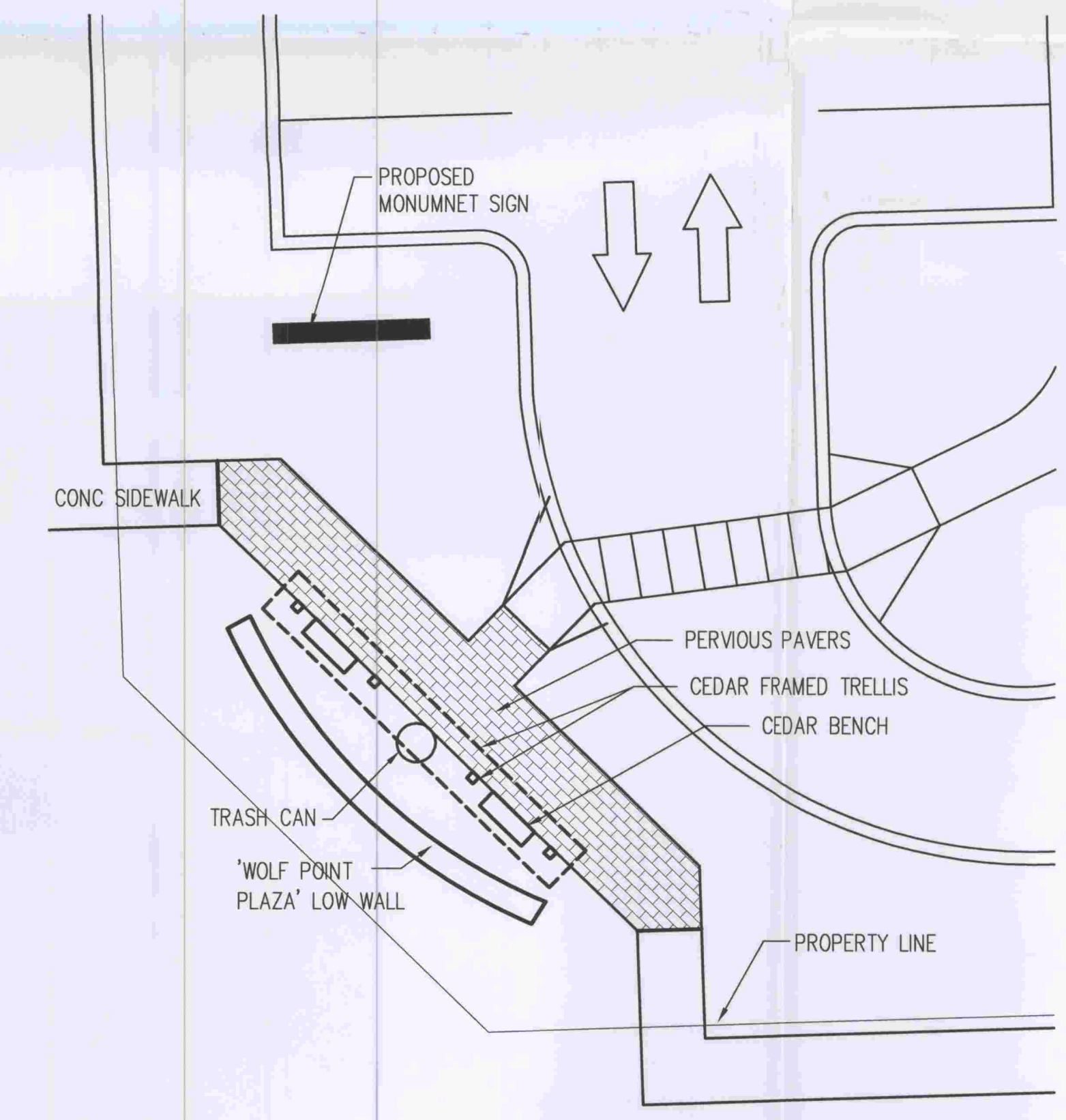


LITTER RECEPTACLE - MODEL TH-24, GOBLET
SERIES WITH DOME LID BY VICTOR STANLEY, INC.
COLOR: METAL - GLOSS BLACK, SLATS - WALNUT



BENCH - MODEL C-140 CLASSIC SERIES BENCH
BY VICTOR STANLEY, INC.
COLOR: FRAME - GLOSS BLACK, SLATS - WALNUT

BOARD APPROVED
CASE NO. 8008-0422
DATE 4-6-09
W/CONDITIONS
W/CONDITIONS



SITE PLAN DETAIL
SCALE: 1" = 10'-0"

**RETAIL DEVELOPMENT
ORLAND PARK, IL
N.E.C. OF 159TH ST AND WOLF RD
AKTON REALTY GROUP**

REVISIONS	

DRAWN	MDI
APPROVED	WCM
ISSUED FOR REVIEW	
DATE	1-16-09
FIELD BOOK	

SHEET NAME
SITE DETAILS

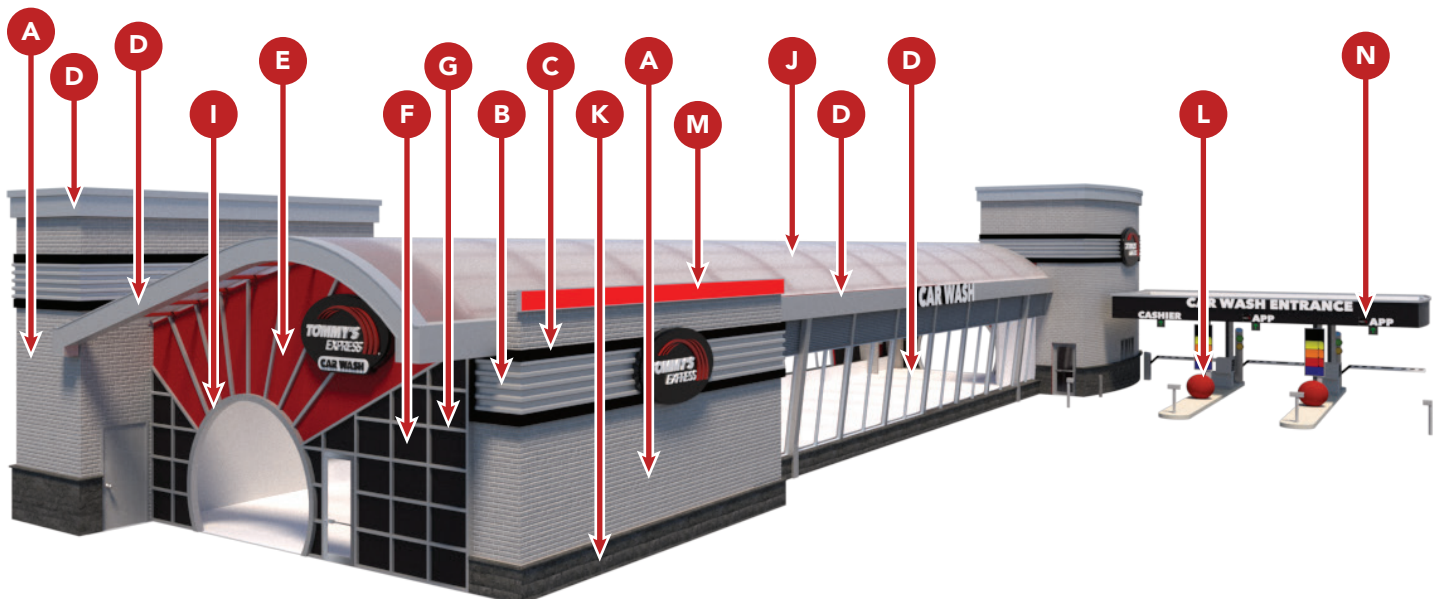
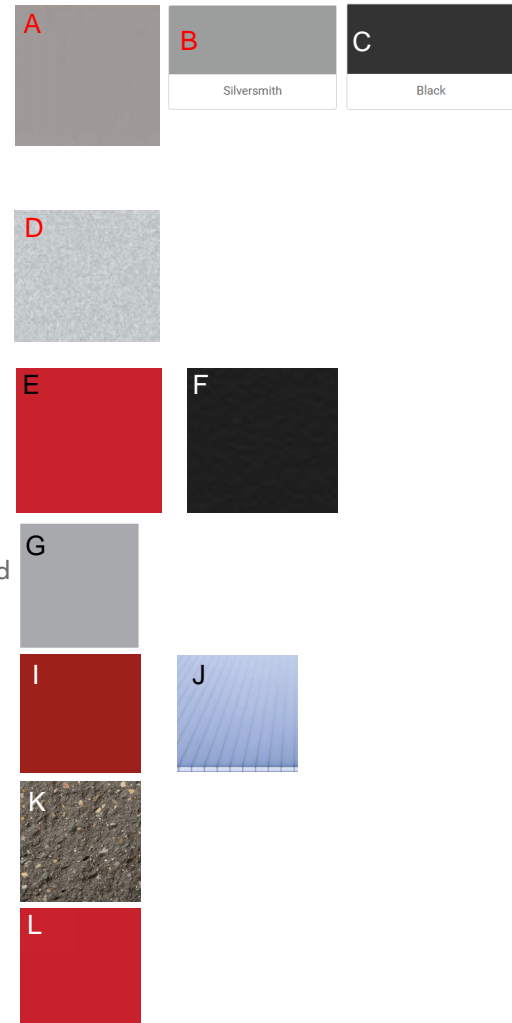
Exterior Material Selections

Tommy's Express Model

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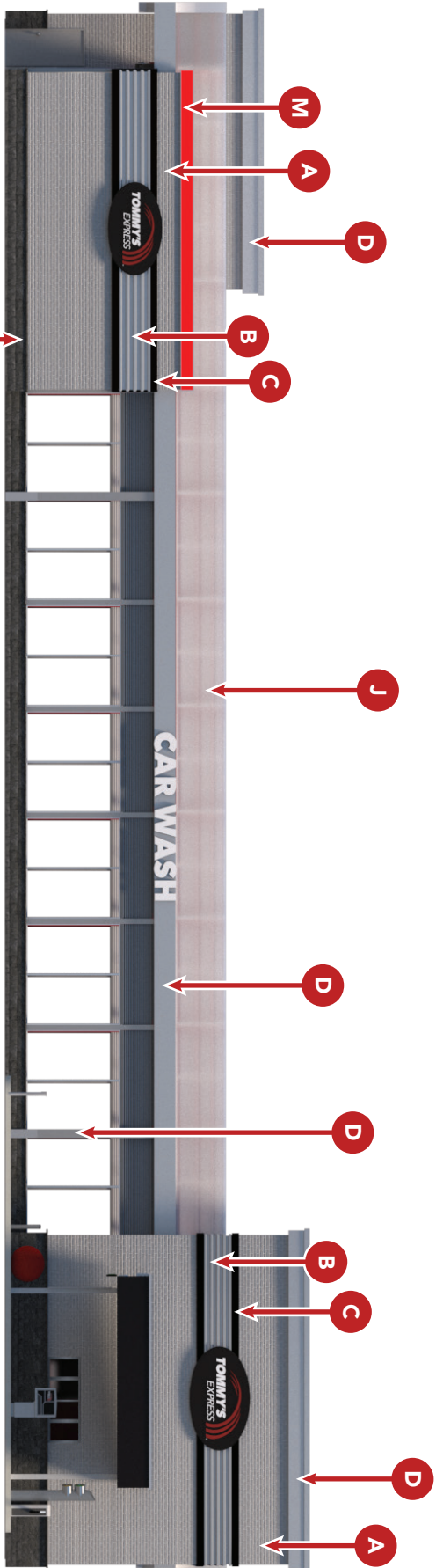
- A** Towers: Glen-Gary - Klaycoat Brick - Stone Gray
- B** Ribbed Panel: ATAS 7.2 - Silversmith
- C** Ribbed Panel Trim: ATAS Flat Sheet - Black
- D** Parapet Cap: Reynolux - Bright Silver Metallic
- D** Fascia: Silver ACM
- D** Downspout Covers: Silver ACM
- E** Car Wash Ends (Upper): Laminators Omega Series - Sunset Red
- F** Car Wash Ends (Lower): Citadel Glaze Guard - Ebony (Series F)
- G** Car Wash Ends (Aluminum Caps): Tubelite 200 Series Curtain Wall - Clear Anodized
- A** Dry Backroom: Glen-Gary - Klaycoat Brick - Stone Gray
- I** Steel: RAL3001
- J** Roof: Acrylic - 8mm ACRYLITE Heatstop - Cool Blue
- K** Masonry - Consumer's 4" Split Faced Block - Ash
- L** Vacuums & Balls: Red
- M** Dry Backroom Parapet: Lektron LED Linear Light - Leon - Red
- N** Pay Canopy: Black ACM



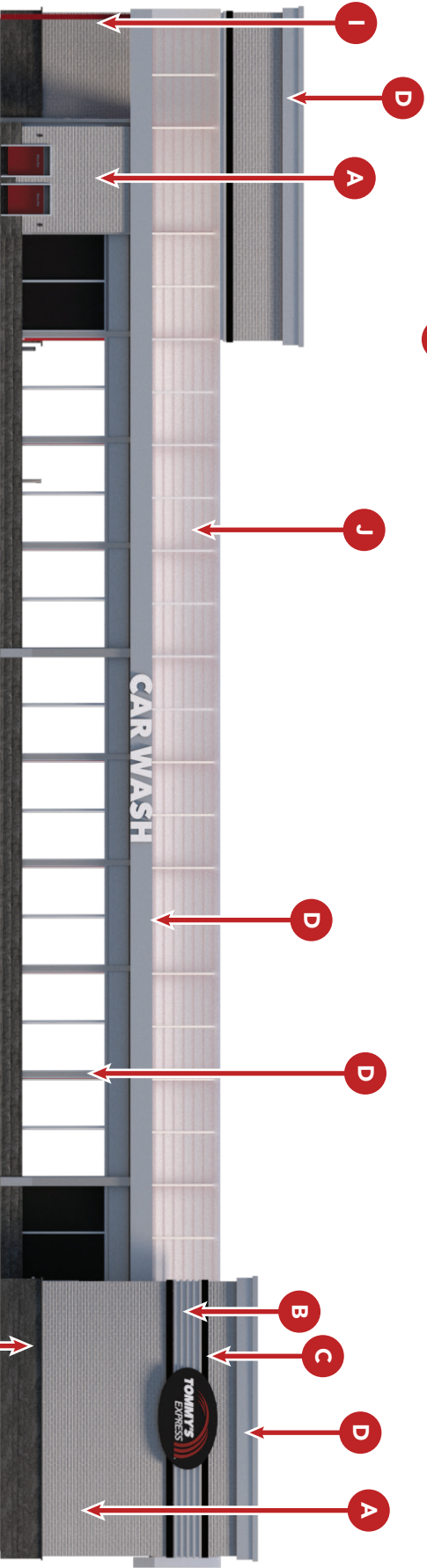
Exterior Material Selections

Tommy's Express Model

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Front Elevation (WEST)



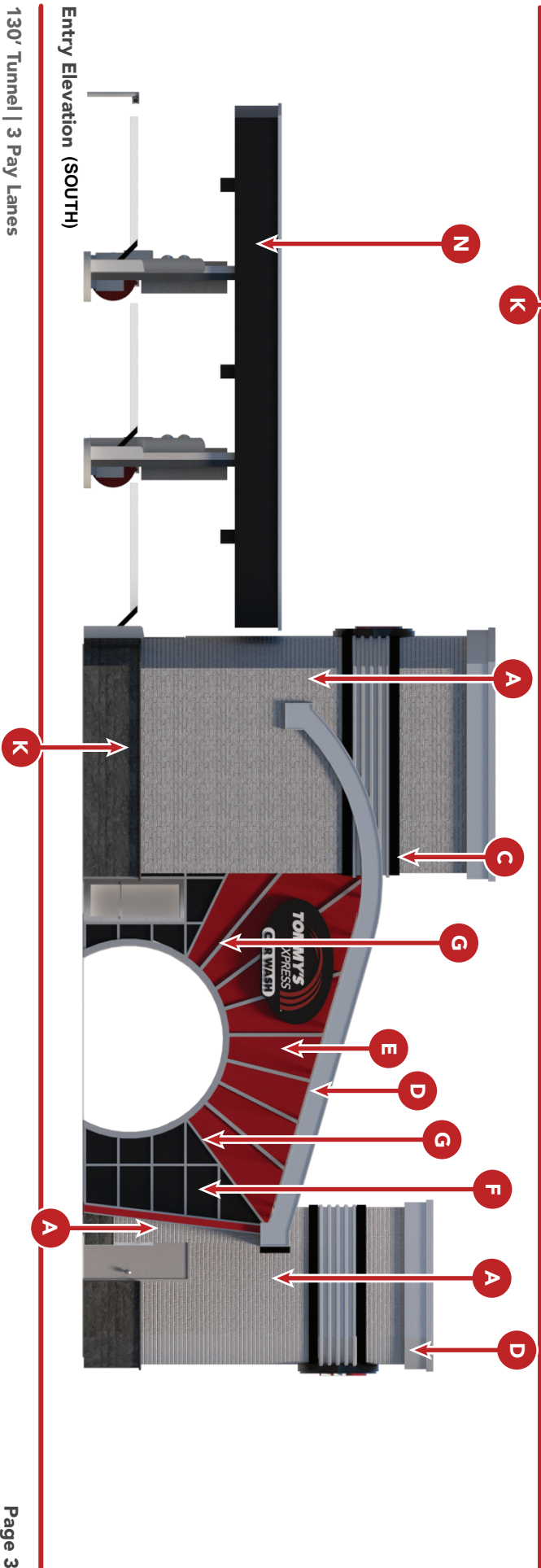
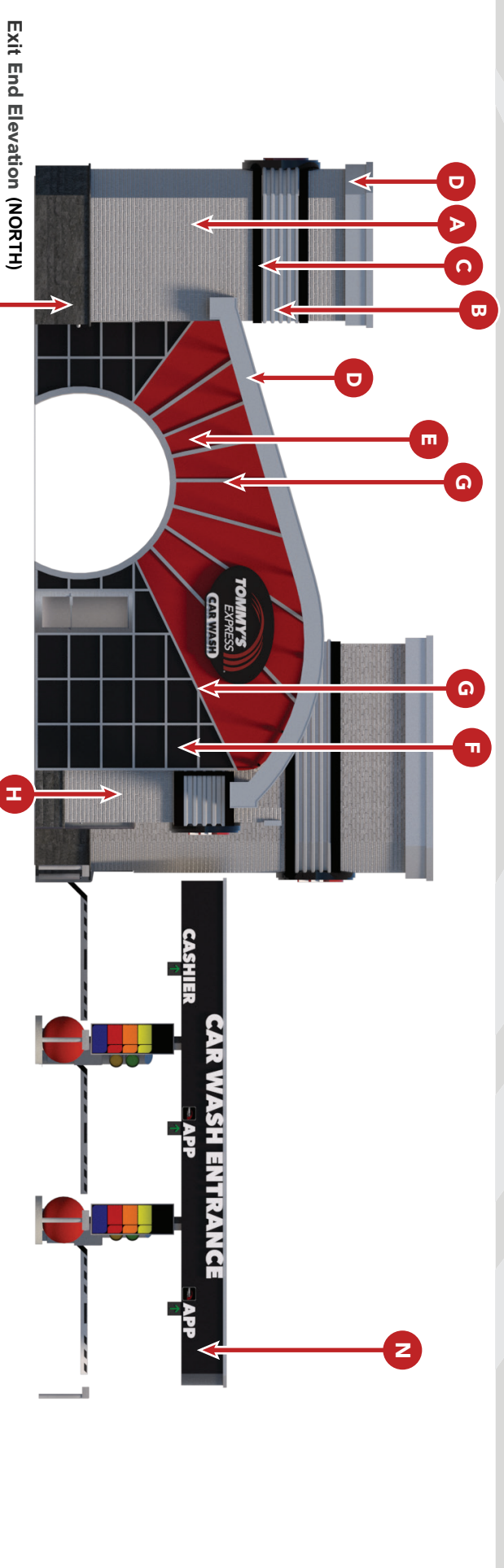
Back Elevation (EAST)

130' Tunnel | 3 Pay Lanes

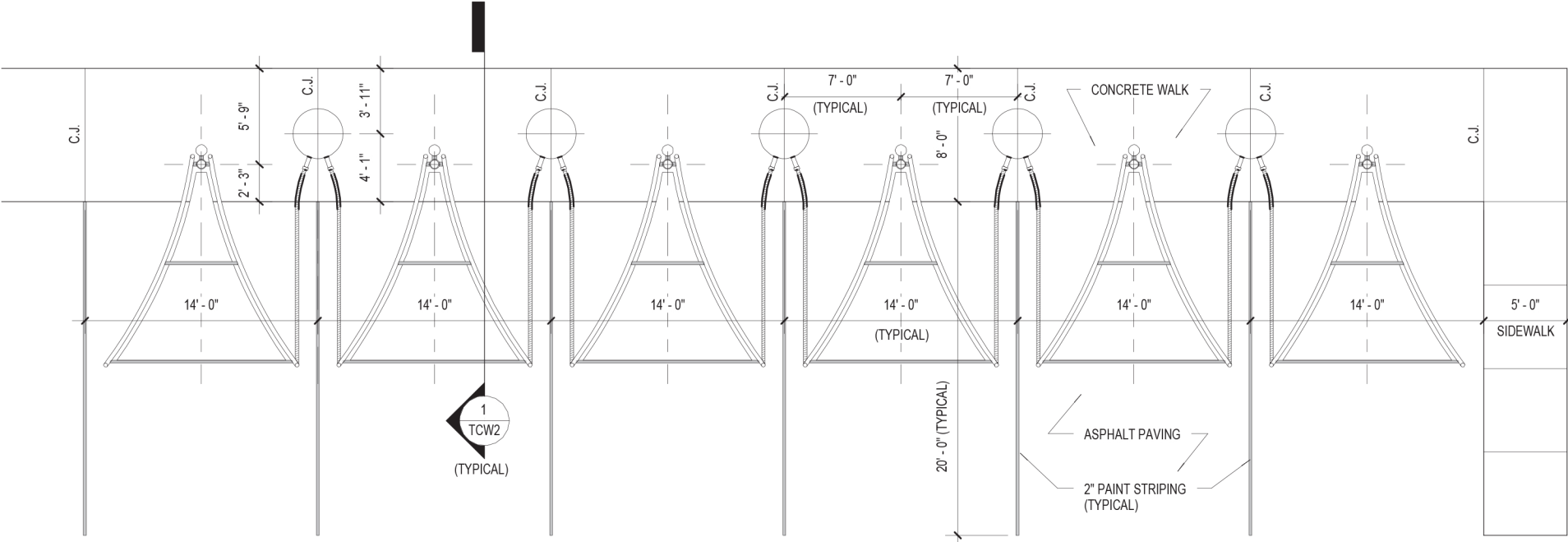
Exterior Material Selections

Tommy's Express Model

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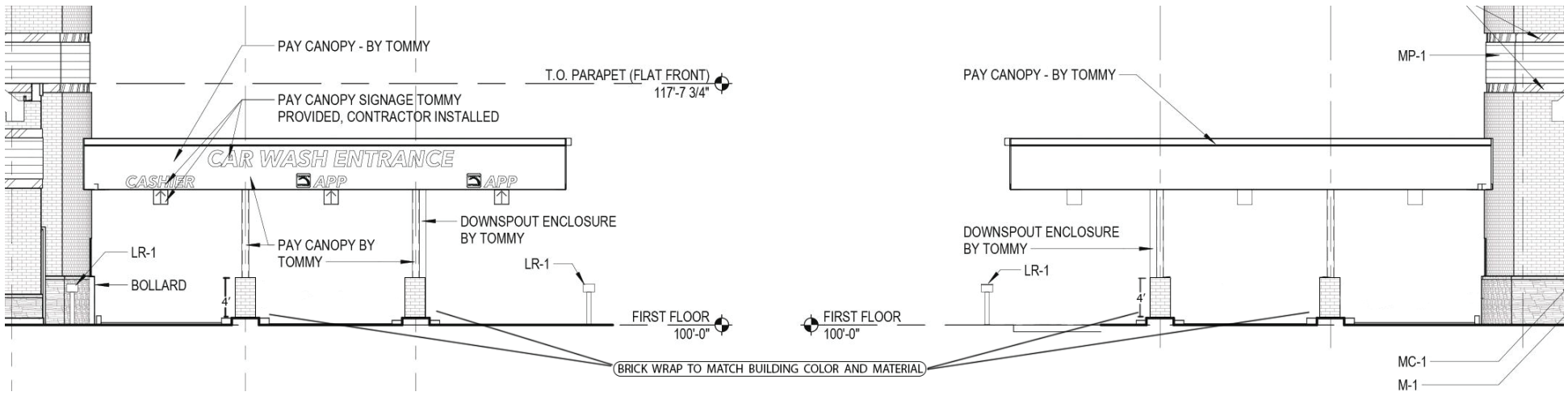
VACUUM FOUNDATION DETAIL



NUMBER OF VACUUM SPACES VARIES
SEE SPECIFIC PROJECT REQUIREMENTS.

Customer **P2146**
 Location **Orland Park**
 Date **03/10/19**
 Revision **1**

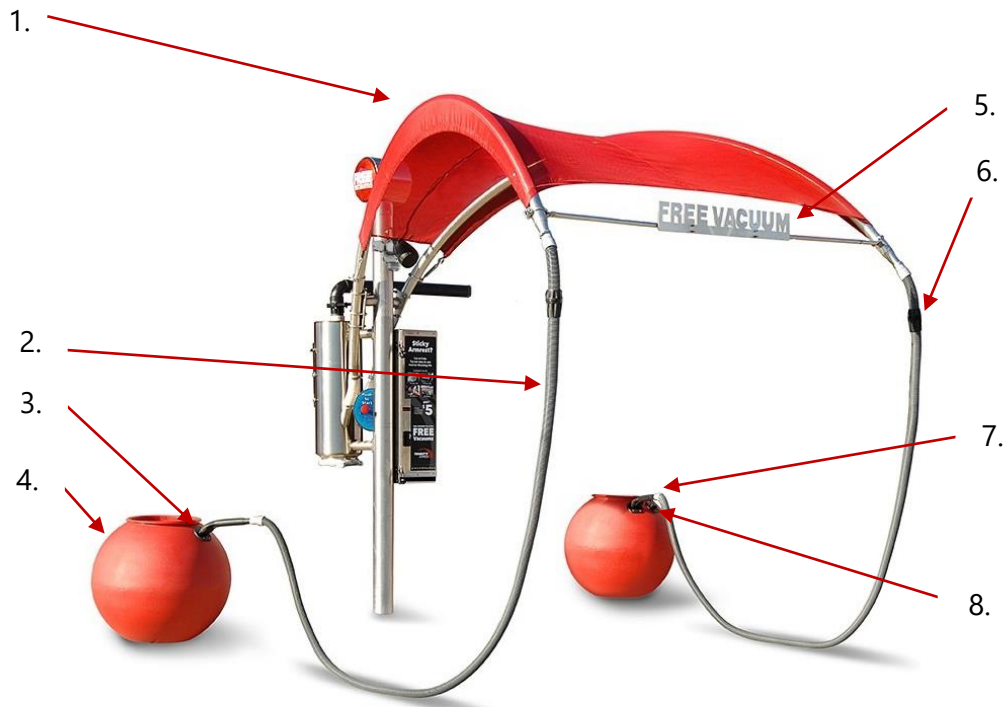
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Pay Canopy Columns

Brick to match Building - 4' from ground

Equipment Components



- | | | |
|----|---------------|-----------------------------------|
| 1. | P-VAC-CPY | Canopy |
| 2. | P-VAC-208 | Tapered Hose |
| | • P-VAC-208-K | Hose Assembly (includes fittings) |
| 3. | P-VAC-105HLSR | Crevice Tool Holster |
| 4. | A-Ball-TX | Tommy Ball |
| 5. | P-VAC-201 | "Free Vacuum" Letters |
| 6. | P-VAC-220 | Swivel Cuff |
| 7. | P-VAC-224 | Vacuum Adapter Cuff - 1.5" x 1.5" |
| 8. | P-VAC-105NZL | Crevice Tool |

TEXTURED TOMMY BALL

A-BALL-TX



Product Description

The Tommy Ball is a fun new take on facility accents with a number of different practical applications around the car wash. The spherical shape matches Totally Tommy starbursts and the arches found in Tommy wash equipment and vacuum stations and provides an attractive and memorable visual reference point for passing customers. Class up your wash and try a new and fun look with the Textured Tommy Ball!

Uses include:

- Trash Cans
- Hose Holders
- Planters
- Highly visible traffic guides

Specifications

- 33" Diameter
- 23 lbs. weight for solid ball



XRS Extreme Roll Up Door 13'5" x 10' (New Version)

Part Number: AIRLIFT-XRS-13X10

Description: 13'5" X 10" Roll-Up Door for Totally Tommy Car Wash Building

Site Map: Tommy Store > In the Tunnel > Garage Doors

Price: \$8,240.71

Detailed Description

The XRS Extreme Roll Up Door keeps your car wash safe, secure, and stylish during inactivity. Built for fast, low-maintenance operation the 13' 5" x 10' Elite Model is sized for the official Totally Tommy building and dual belt conveyor. Features waterproofing, stainless steel hardware, infrared safety eyes, and a basic control option.

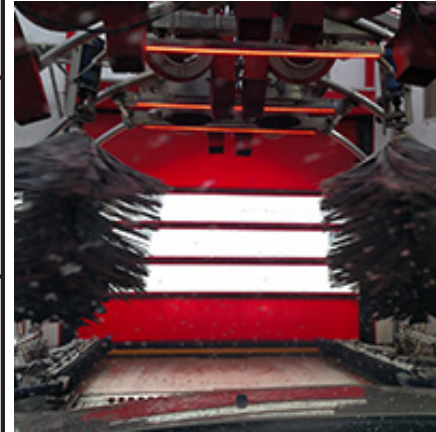
Specifications

- No Counterweights
- Elite Model
- 13'5" x 10'
- 240v 3 Phase Motor
- 110v Single Phase Controller
- Speeds Adjustable up to 34" Per Second
- Corrosion Resistant Components

[Owners Manual](#)

Features

- Breakaway Design with Automatic Reset
- Complete Weather Seal Including: Heavy Duty Top Rubber Seal and Dual Side Brush Seals
- Simple, Low Maintenance Design
- Individually Replaceable Components



Rhino-Mat Floor Mat Cleaners

Part Number: E-DET-205

Description: Automatic Car and Truck Floor Mat Cleaners for Full or Self Service Car Washes - All Weather Stainless Steel - Select Cleaning and Payment System

Site Map: Tommy Store > Vacuum and Vending > Mat Washers

Price: Go Online for Price



Detailed Description

Drastically improve your full service detailing times or offer improved on-site self-service cleaning with Rhino-Mat Automatic Floor Mat Cleaners.

RHM-SS : Auto Wash & Dry System with Push Button Start

RHM-SSBC : Auto Wash & Dry System with Bill and Coin Acceptor

This ground mounted cleaner is ETL certified for safe and hazard free operation and can completely clean and dry most plastic or fabric car wash mats in thirty seconds. All weather stainless steel construction includes a heating element for year-round operation in northern climates while a state-of-the-art cleaning system is able to purge oil, hair, sand, mud, and more from all standard floor mats in just seconds. Just insert the mat and let the cleaner do the rest!

Specifications

- Weight RHM-SSBC : 500 lbs
- Height RHM-SSBC : 50"
- Width RHM-SSBC : 40"
- Depth RHM-SSBC : 26"
- Power RHM-SSBC : 110 V/60 Hz (30 Amp Rqmt.)
- Processing Rate RHM-SSBC : 30 seconds / Mat

Features

- Absorbent sponge and steel rollers to squeeze out moisture
- Patented extraction tray which will dry the mats complete to touch
- Automatic floor mat cleaning solution for carpet & rubber mats
- One-step mat washing & drying process. Save on labor & time
- Washes and dries a vehicle mat within 30 seconds
- Removes ground-in dirt, oil, sand, pet hair and most stains
- Auto-flat feed operation to prevent folding or damage to a mat
- Safe & easy operation and maintenance
- Integrated auto heating system made for year-round use
- Control board system
- Automatic shut-off timer, time adjustable & energy efficient
- Auto-counter, keep track of the number of mats you clean

Price as of 11/20/18 Large rollers with wheels for easy movement with optional ground mounting

Customer -
Location **159th/Wolf - Orland Park, IL**
Date **03/28/19**
Revision **11**

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Customer -
Location **159th/Wolf - Orland Park, IL**
Date **03/28/19**
Revision **11**

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Site Plan

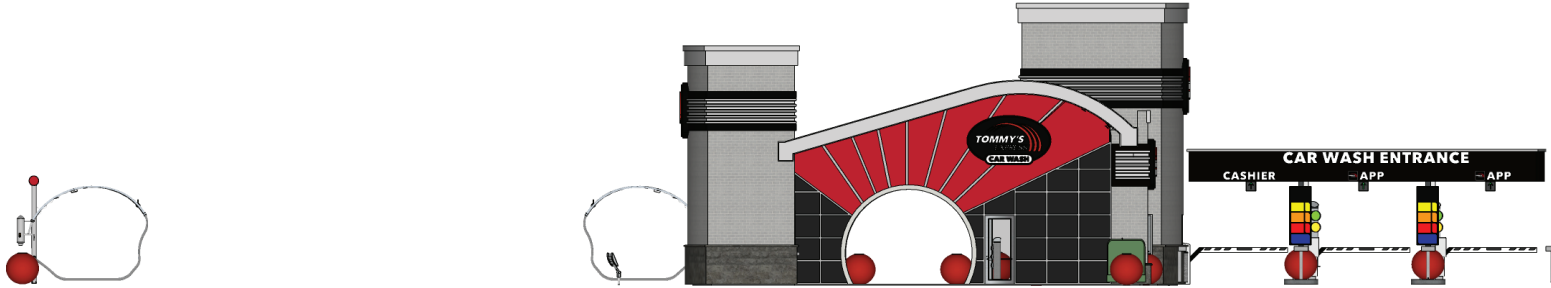
130' Tunnel | 16 Vacuums | 3 Pay Lanes

Customer -
Location 159th/Wolf - Orland Park, IL
Date 03/28/19
Revision 11

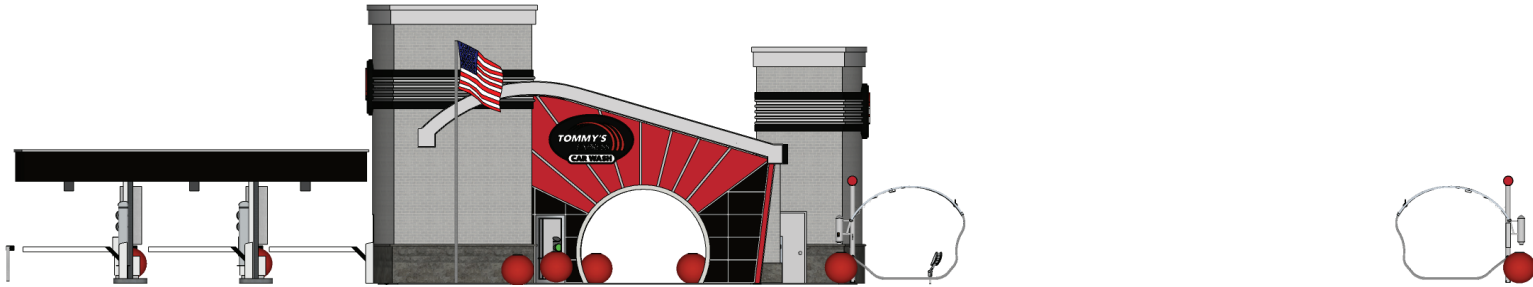
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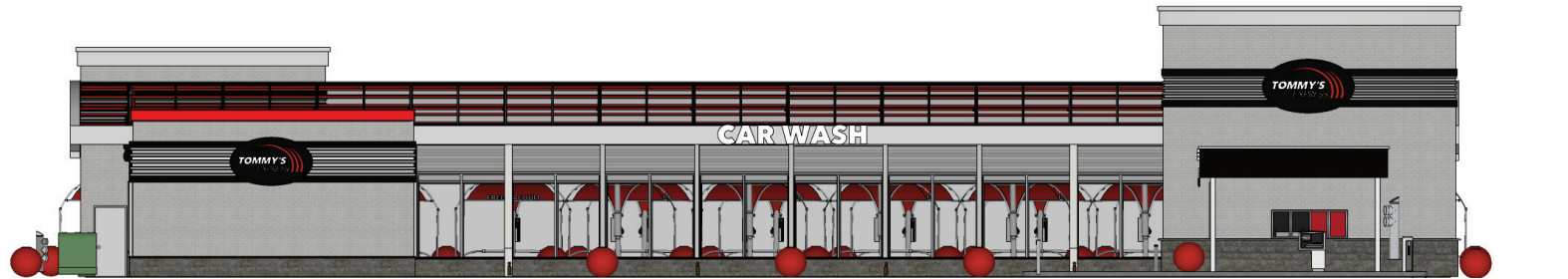
S - Exit End Elevation



N - Entry End Elevation



W - Front Elevation



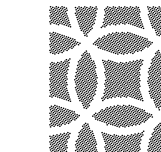
E - Back Elevation



Customer -
Location **159th/Wolf - Orland Park, IL**
Date **03/28/19**
Revision **11**

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**DANIEL WEINBACH
&
PARTNERS, LTD.**

Landscape Architects

53 W. Jackson Blvd.
Suite 250
Chicago, IL 60604

312 427-2888
www.dwppltd.com

Revisions

8.		
7.		
6.		
5.		
4.		
3.	REVISED PER VILLAGE COMMENTS	02/12/19
2.	REVISED PER VILLAGE COMMENTS	11/30/18
1.	ISSUED FOR REVIEW	08/31/18

Project

**TOMMY'S CAR
WASH**

STREET ADDRESS
CHICAGO, ILLINOIS

Sheet Title

**TREE
MITIGATION PLAN**

Date 08/20/18	Project No. DWP 18-179
Scale 1" = 20'-0"	Sheet No. L-1.0
Drawn By RT	
Approved WS	

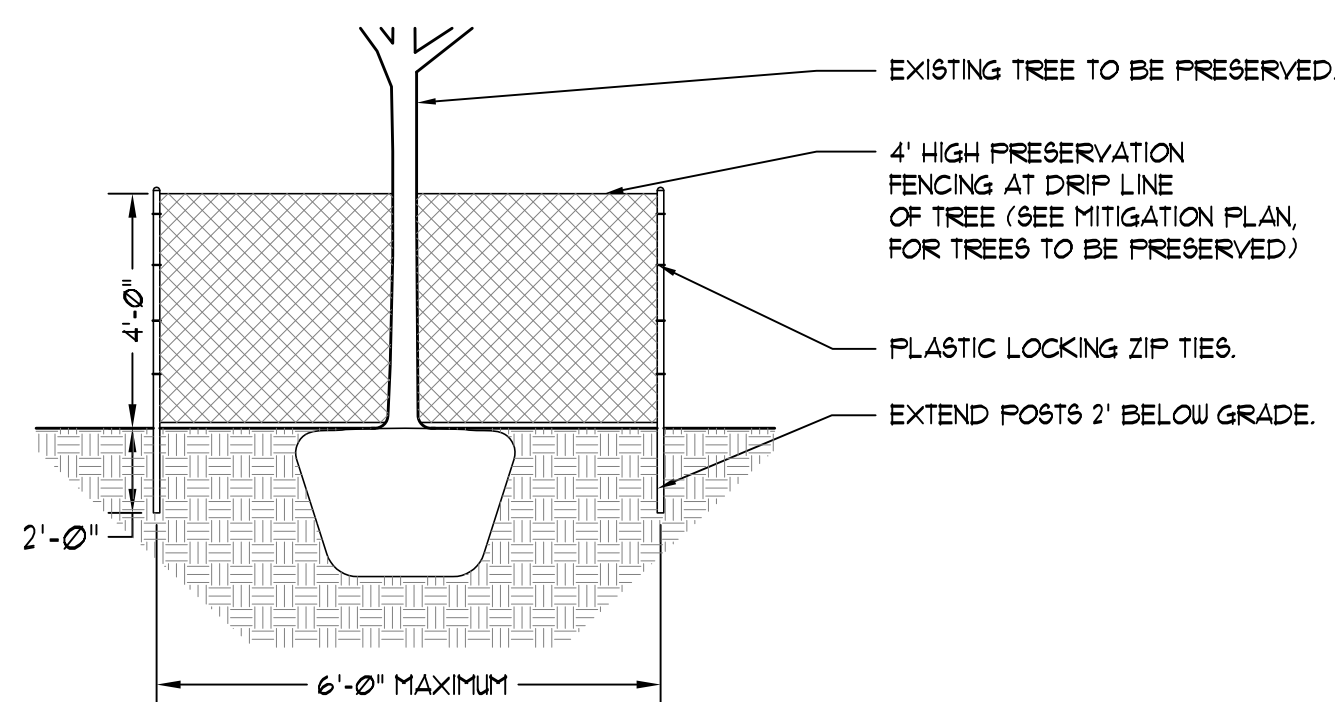
TREE PROTECTION NOTES

- ALL EXISTING TREE LOCATIONS AND SIZES ARE TO BE VERIFIED IN THE FIELD BY THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES BETWEEN THE DRAWINGS AND ACTUAL FIELD CONDITIONS, PRIOR TO THE START OF CONSTRUCTION.
- ALL TREES TO REMAIN AS NOTED ON THE DRAWINGS ARE TO BE ENCLOSED WITH A TREE PROTECTION FENCE, PRIOR TO THE START OF CONSTRUCTION. THE FENCE SHALL CONSIST OF 4' TALL ORANGE PLASTIC FENCE WITH METAL POSTS SPACED NO FURTHER THAN 6' APART. THE FENCE IS TO BE LOCATED UNDER THE OUTER EDGES OF THE TREE'S CANOPY, BUT NO CLOSER THAN 5' AWAY FROM THE TREE'S TRUNK. LANDSCAPE ARCHITECT TO APPROVE FENCE LOCATION.
- NO MATERIALS OR VEHICLES SHALL BE STORED, DRIVEN OR PARKED WITHIN TREE PROTECTION FENCING.
- ALL GRADING, CONSTRUCTION EQUIPMENT AND MATERIALS SHALL BE FORBIDDEN FROM ENCRDACHING WITHIN THE TREE PROTECTION FENCING.
- CRUSHED LIMESTONE OR ANY MATERIALS THAT MAY BE DETRIMENTAL TO TREES SHALL NOT BE DUMPED WITHIN THE TREE PROTECTION FENCING NOR SHALL THEY BE LOCATED AT ANY HIGHER ELEVATION WHERE DRAINAGE TOWARD THE TREE(S) COULD CONCEIVABLY AFFECT THE HEALTH OF SAID TREE(S).
- IF IT IS THE CONTRACTOR'S OPINION THAT ANY OF THE TREES TO BE PRESERVED WILL BE NEGATIVELY AFFECTED DURING THE CONSTRUCTION PROCESS, THE LANDSCAPE ARCHITECT SHALL BE NOTIFIED IMMEDIATELY.
- THE CONTRACTOR IS TO MAINTAIN THE PROTECTION FENCING THROUGHOUT THE ENTIRE CONSTRUCTION PROCESS UNTIL NEW LANDSCAPING BEGINS AND THE LANDSCAPE ARCHITECT APPROVES REMOVAL OF THE FENCING.
- IF ROOTS OF A TREE TO REMAIN ARE EXPOSED, THE LANDSCAPE ARCHITECT IS TO BE CONTACTED AT ONCE.
- ALL TREES TO BE PRESERVED SHALL BE PRUNED TO REMOVE EXISTING DEAD WOOD TO COMPENSATE FOR ROOT LOSS DURING CONSTRUCTION BY A QUALIFIED ARBORIST WITH A MINIMUM 5 YEARS EXPERIENCE AT THE DIRECTION OF THE LANDSCAPE ARCHITECT.
- IF THE CONTRACTOR DAMAGES A TREE TO BE PROTECTED SO THAT IT DOES NOT SURVIVE HE SHALL BE RESPONSIBLE FOR REPLACING THE TREE IN KIND OR REPLACING IT WITH TREES ABOUT THE SAME NUMBER OF CALIFER INCHES (MEASURED 6" ABOVE THE GROUND.) THE LANDSCAPE ARCHITECT IS TO APPROVE THE REPLACEMENT TREES.

EXISTING TREES

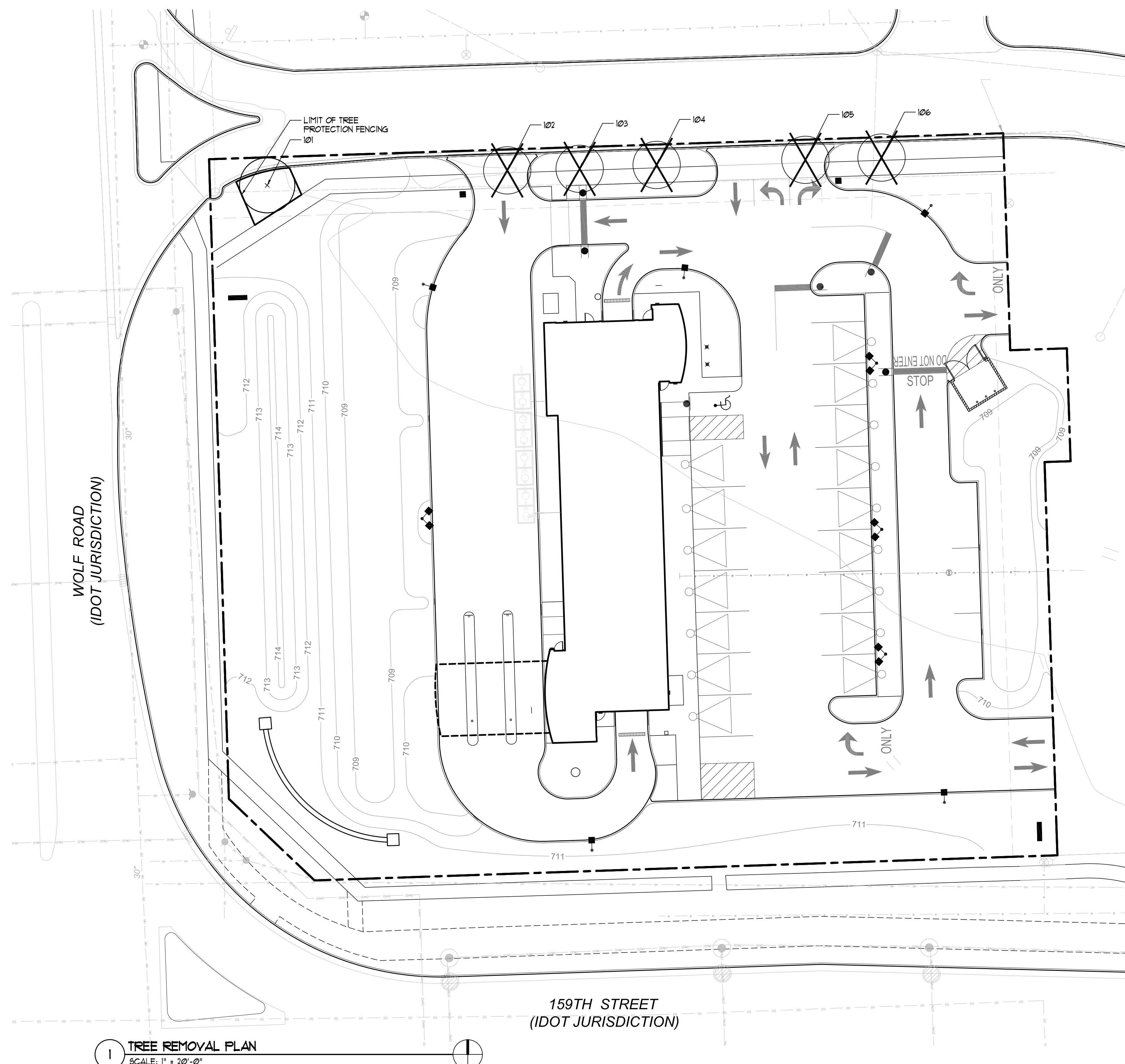
CODE	BOTANICAL NAME	SIZE	CONDITION
101	SHADE TREE	25" CAL	GOOD
102	SHADE TREE	25" CAL	GOOD
103	SHADE TREE	25" CAL	GOOD
104	SHADE TREE	25" CAL	GOOD
105	SHADE TREE	25" CAL	GOOD
106	SHADE TREE	25" CAL	GOOD

FENCING SHALL BE LOCATED AS SHOWN ON THE TREE MITIGATION PLAN ON THIS SHEET. FENCE SHALL BE PLACED SO THAT THE POSTS FACE THE TREE. AFTER INSTALLING ALL POSTS, TIGHTLY WRAP THE FABRIC ACROSS THE POSTS TO PREVENT SAGGING. POSTS SHALL BE PLACED A MAXIMUM OF 6' ON CENTER. SAFETY FENCE FABRIC SHALL BE ORANGE CONTRACTOR'S GRADE SMOOTH TOP DIAMOND PLASTIC FENCE. POSTS SHALL BE 6' LONG HEAVY DUTY GALVANIZED STEEL T-POSTS, SET TO A DEPTH OF 2' BELOW GRADE. POST SHALL NOT BE DRIVEN INTO EXISTING ROOTS. LOCKING TIES SHALL BE 8" PLASTIC LOCKING ZIP TIES, 4 PER POST. ANY ALTERATIONS TO THE FENCING LAYOUT MUST BE APPROVED BY LANDSCAPE ARCHITECT.



2 TREE PRESERVATION DETAIL

NOT TO SCALE



1 TREE REMOVAL PLAN

SCALE: 1" = 20'-0"

PLANT LIST

CODE	BOTANICAL NAME	COMMON NAME	SIZE	QUANT.	ADDITIONAL NOTES	% OF TOTAL	NATIVE
DECIDUOUS TREES - SHADE							
AM	ACER MIMABEI STATE STREET MORTON'	STATE STREET MAPLE	25" CALIPER	6	BRANCHED UP 6'	16.1%	YES
CO	CELTIS OCCIDENTALIS	COMMON HACKBERRY	25" CALIPER	3	BRANCHED UP 6'	8.3%	YES
GB	GINKGO BILOBA 'AUTUMN GOLD'	AUTUMN GOLD GINKGO	25" CALIPER	1	BRANCHED UP 6'	19.4%	NO
GD	GYMNOCLADUS DIOICA	KENTUCKY COFFEETREE	25" CALIPER	2	BRANCHED UP 6'	5.5%	YES
GT	GLEDITSIA T. VAR. INERMIS 'SKYLINE'	SKYLINE HONEYLOCUST	25" CALIPER	5	BRANCHED UP 6'	13.9%	YES
PC	PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	25" CALIPER	7	BRANCHED UP 6'	5.5%	NO
QB	QUERCUS BICOLOR	SWAMP WHITE OAK	25" CALIPER	5	BRANCHED UP 6'	13.9%	YES
UT	ULMUS x MORTON GLOSSY'	TRIUMPH ELM	25" CALIPER	6	BRANCHED UP 6'	16.1%	NO
DECIDUOUS TREES - ORNAMENTAL							
AG	AMELANCHIER G. 'AUTUMN BRILLIANCE'	APPLE SERVICEBERRY	6' HT	3	CLUMP FORM	30.0%	YES
MR	MALUS 'RED JEWEL'	RED JEWEL CRABAPPLE	6' HT	2	CLUMP FORM	20.0%	NO
SR	SYRINGA RETULATA 'IVORY SILK'	IVORY SILK JAPANESE TREE LILAC	6' HT	5	CLUMP FORM	50.0%	NO
DECIDUOUS SHRUBS							
AA	ARONIA MELANOCARPA 'AUTUMN MAGIC'	AUTUMN MAGIC CHOKEBERRY	18" HT x 24" W	22		9.8%	YES
DL	DIERVILLA LONICERA	DWARF BUSH HONEYSUCKLE	18" HT x 24" W	20		8.8%	YES
DR	DIERVILLA RIVULAROS 'KODIAK ORANGE'	KODIAK ORANGE HONEYSUCKLE	18" HT x 24" W	21		9.3%	NO
HK	HYPERICUM KALMIANUM	KALM ST. JOHNSWORT	24" HT x 14" W	12		5.3%	YES
RA	RHUS AROMATICA 'GRO-LOW'	GRO-LOW SUMAC	18" HT x 24" W	31		13.2%	YES
SA	SPIRAEA ALBA	MEADOWSWEET	24" HT x 24" W	20		8.8%	YES
SM	SYRINGA MEYERI 'PALIBIN'	DWARF KOREAN LILAC	30" HT x 30" W	25		11.0%	NO
VC	VIBURNUM CARLESII 'CAYUGA'	CAYUGA KOREANSPICE VIBURNUM	30" HT x 30" W	11		5.0%	NO
EVERGREEN SHRUBS							
JB	JUNIPERUS HORIZONTALIS 'BLUE CHIP'	BLUE CHIP JUNIPER	18" HT x 24" W	49		21.1%	YES
TM	TAXUS x MEDIA 'TAUNTON'	TAUNTON DENSE YEW	24" HT x 24" W	13		5.8%	NO
PERENNIALS, GROUNDCOVERS & ORNAMENTAL GRASSES							
GA	GALLARDA ARISETATA 'ARIZONA SUN'	ARIZONA SUN BLANKET FLOWER	1 GALLON	120	SPACED 24" O.C.	24.1% FER	YES
SS	SALVIA SYLVESTRIS 'MAY NIGHT'	MAY NIGHT SALVIA	1 GALLON	78	SPACED 18" O.C.	19.6% FER	NO
HM	HEMEROCALLIS MIX (50/50 MIX)	DAYLILY 'HAPPY RETURNS'	1 GALLON	32	SPACED 18" O.C.	6.4% FER	NO
		DAYLILY 'LITTLE WINE CUP'	1 GALLON	32	EVENLY MIXED	6.4% FER	NO
LK	LIATRIS 'KOBOLD'	COMPACT GAFFEAHER	1 GALLON	53	SPACED 12" O.C.	10.6% FER	YES
CG	COREOPSIS GRANDIFLORA	LARGE FLOURED TICKSEED	1 GALLON	11	SPACED 18" O.C.	15.4% FER	YES
FV	PANICUM VIRGATUM	SWITCH GRASS	1 GALLON	44	SPACED 24" O.C.	11.3% GRASS	YES
CL	CHASMANTHUM LATIFOLIUM	NORTHERN SEA OATS	1 GALLON	19	SPACED 18" O.C.	4.8% GRASS	YES
DC	DESCHAMPSIA CESPIIOSA 'PIXIE FOUNTAIN'	PIXIE FOUNTAIN TUFTED HAIR GRASS	1 GALLON	11	SPACED 18" O.C.	19.1% GRASS	YES
PA	FENNICETUM ALOPECUROIDES 'PIGLET'	PIGLET FOUNTAIN GRASS	1 GALLON	58	SPACED 18" O.C.	14.8% GRASS	YES
ES	ERAGROSTIS SPECTABILIS	PURPLE LOVE GRASS	1 GALLON	124	SPACED 15" O.C.	29.5% GRASS	YES
SEM	SPOROPOLUS HETEROPELLIS & ECHINACEA 'CEG CONE 2' (50/50 MIX)	PRAIRIE DROPSSEED PIXIE MEADOWBRIT CONEFLOWER	1 GALLON	21	SPACED 24" O.C.	6.9% GRASS	YES
			1 GALLON	21	EVENLY MIXED	5.4% FER	YES
BC	BOUTELOUA CURTIFENDULA	SIDEOATS GRAMA	1 GALLON	20	SPACED 24" O.C.	5.1% GRASS	YES
CA	CALAMAGROSTIS ACUTIFLORA 'OVERDAM'	OVERDAM FEATHER REED GRASS	1 GALLON	21	SPACED 18" O.C.	1.1% GRASS	NO
EM	ECHINACEA PURPUREA 'MAGNUS'	MAGNUS PURPLE CONEFLOWER	1 GALLON	50	SPACED 18" O.C.	10.1% FER	YES
NF	NEPETA FAASSENII PURRSIAN BLUE	FURRSIAN BLUE CATMINT	1 GALLON	30	SPACED 18" O.C.	6.1% FER	NO

DATA BOX - GREENSPACE

TOTAL LOT AREA	66,389 SF
TOTAL LANDSCAPE AREA	28,190 SF
TOTAL # PARKING STALLS	19
TOTAL LANDSCAPE ISLAND AREA	392 SF

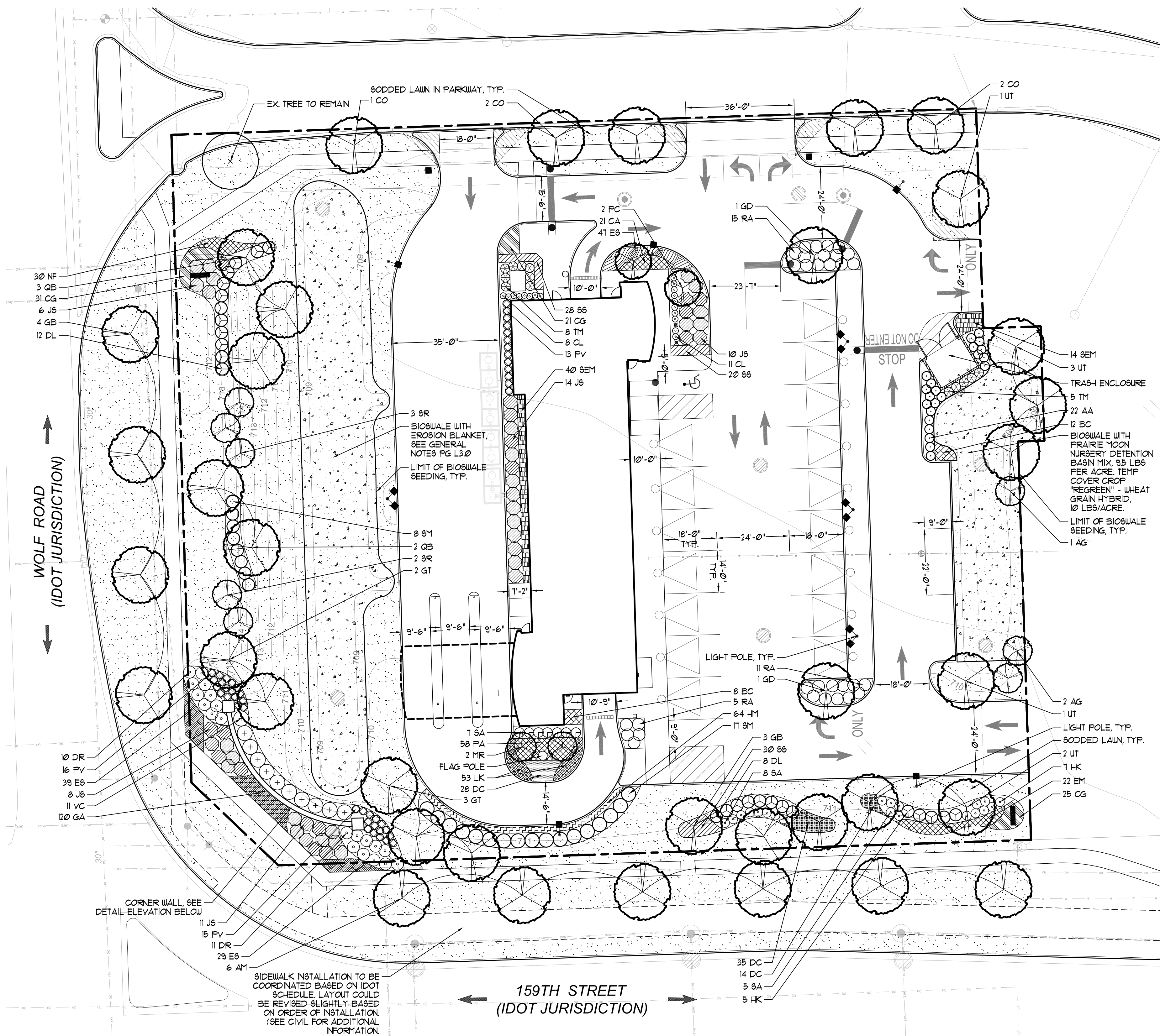
DATA BOX - REQUIRED LANDSCAPE MATERIALS PER ZONE

AREA	REQUIRED TREES	PROVIDED TREES
CORRIDOR	SHADE TREE - 15 ORNAMENTAL TREE - 5	SHADE TREE - 15 ORNAMENTAL TREE - 5 SHRUB - 121
PARKWAY	SHADE TREE - 16	SHADE TREE - 16
BUFFER	SHADE TREE - 5 ORNAMENTAL TREE - 3 SHRUB - 21	SHADE TREE - 5 ORNAMENTAL TREE - 3 SHRUB - 21
BUILDING	ORNAMENTAL TREE - 4	ORNAMENTAL TREE - 4 SHRUB - 44
PARKING LOT	SHADE TREE - 2	SHADE TREE - 2 SHRUB - 26
TOTAL LOT	SHADE TREE - 36 ORNAMENTAL TREE - 12 SHRUB - 21	SHADE TREE - 38 ORNAMENTAL TREE - 12 SHRUB - 224

DATA BOX - REQUIRED LANDSCAPE SPECIES PER TYPE

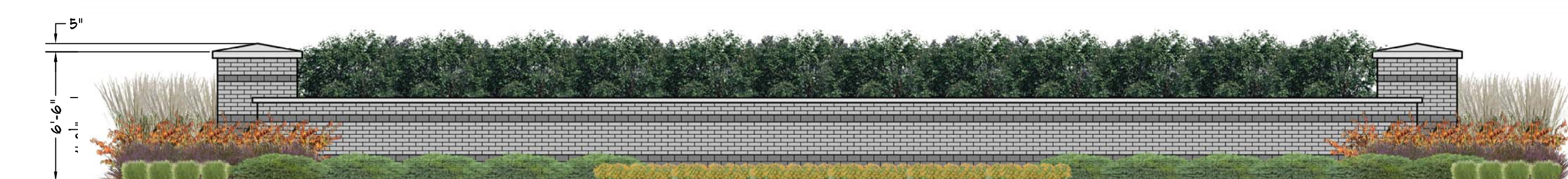
PLANT TYPE	TOTAL NUMBER	VARIETIES REQ.	VARIETIES PROVIDED
SHADE TREE	36	5	8
ORNAMENTAL TREE	10	3	3
SHRUB	224	8	10
PERENNIAL	493	8	9
ORNAMENTAL GRASS	390	8	8

** SEE PLANT LIST FOR VARIETY SPECIFICATIONS



1 LANDSCAPE PLAN
SCALE: 1" = 20'-0"

2 WALL ELEVATION
SCALE: 3/16" = 1'-0"



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Revisions

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3.	REVISED PER VILLAGE COMMENTS 02/12/19
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1.	ISSUED FOR REVIEW 08/31/18

TOMMY'S CAR WASH
159TH ST & WOLF RD
ORLAND PARK, ILLINOIS

LANDSCAPE PLAN

Date	08/31/18	Project No.	DWP 18-179
Scale	1" = 20'-0"	Sheet No.	L-2.0
Drawn By	RT		
Approved	WS		

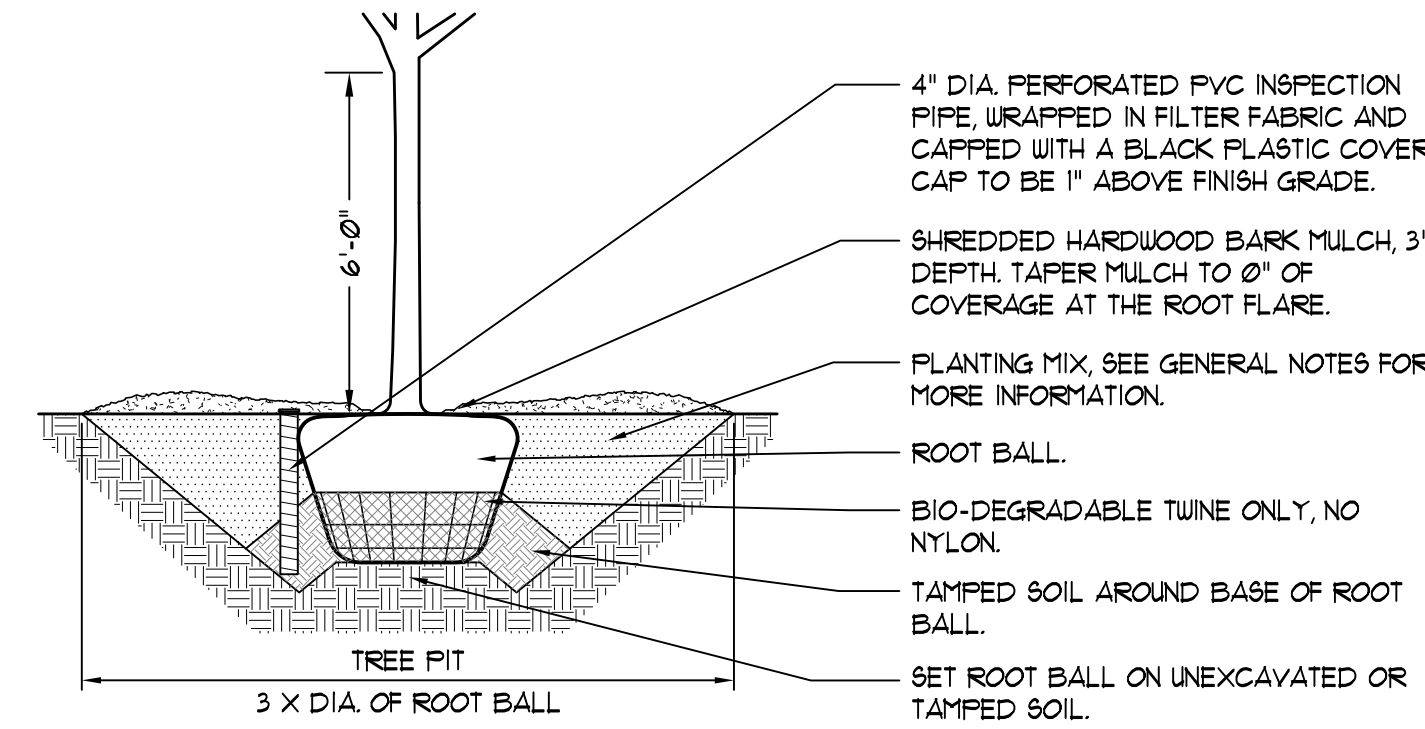
GENERAL NOTES

- ALL DIMENSIONS ARE TO BE VERIFIED IN THE FIELD BY THE CONTRACTOR. NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE YEARS EXPERIENCE WITH PROJECTS OF SIMILAR SIZE AND TYPE.
- SPECIES AND SIZES OF PLANTS LISTED IN THE PLANT LIST ARE SUBJECT TO AVAILABILITY AT TIME OF INSTALLATION. IF SUBSTITUTIONS ARE REQUIRED, THE CONTRACTOR SHALL SUBMIT REQUESTS TO THE LANDSCAPE ARCHITECT FOR APPROVAL.
- QUANTITIES SHOWN ON THE DRAWING AND IN THE PLANT LIST ARE PROVIDED ONLY FOR THE CONTRACTOR'S CONVENIENCE. THE NUMBER OF PLANT SYMBOLS SHOWN ON THE DRAWINGS SUPERCEDE ALL QUANTITIES PROVIDED. THE CONTRACTOR SHALL VERIFY ALL PLANT QUANTITIES ON THE DRAWINGS AND NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES PRIOR TO INSTALLATION. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLING ALL PLANTS PER THE PLANT SYMBOLS SHOWN ON THE DRAWINGS.
- ALL OVERHEAD AND UNDERGROUND UTILITIES ARE TO BE LOCATED PRIOR TO DIGGING OR EXCAVATION. IF UTILITIES OR OTHER OBSTRUCTIONS ARE DISCOVERED TO CONFLICT WITH GRADING, PLANT OR MATERIALS PLACEMENT, NOTIFY THE LANDSCAPE ARCHITECT SO THAT ADJUSTMENTS MAY BE MADE PRIOR TO PROCEEDING.
- CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT EXISTING PLANTS, LAWN AREA FENCES, SITE FEATURES AND PAVING TO REMAIN. ANY DAMAGE TO THESE EXISTING AREAS OR FEATURES SHALL BE REPLACED BY THE CONTRACTOR AT THEIR EXPENSE. DAMAGED LAWN AREAS SHALL BE TILLED, RE-GRADED, TOPSOIL ADDED AS NEEDED AND BE RESTORED WITH SOD. REPLACED FEATURES, PLANTS AND SOD SHALL BE WARRANTED BY THE CONTRACTOR FOR A PERIOD OF ONE YEAR.
- PROVIDE POSITIVE DRAINAGE AT ALL TIMES. DO NOT OBSTRUCT THE NATURAL OR ENGINEERED DRAINAGE FLOW PATTERN. NOTIFY THE LANDSCAPE ARCHITECT OF ANY DRAINAGE CONCERNS.
- IMPORTED TOPSOIL FOR THIS PROJECT SHALL BE FRIABLE LOAM OR SANDY LOAM, COMPRISED OF BETWEEN 20% AND 50% SILT, 10% AND 25% CLAY, AND 30% AND 60% SAND. THE PH LEVEL SHALL BE BETWEEN 6.0 AND 7.2 AND THE ORGANIC MATTER CONTENT SHALL BE BETWEEN 3% AND 6%. TOPSOIL SHALL BE FREE OF SEEDS, STONES, LARGE CLUMPS, ROOTS, LEAVES AND DEBRIS, AND SHALL NOT BE DELIVERED WHILE IN A FROZEN OR MUDDY CONDITION. CONTRACTOR SHALL PROVIDE A RECENT SOIL TEST ANALYSIS FOR ALL SOIL TO BE USED. THE ANALYSIS MUST INCLUDE THE INFORMATION REQUIRED ABOVE. IF TESTED TOPSOIL REQUIRES AMENDMENTS TO COMPLY WITH REQUIREMENTS, CONTRACTOR SHALL AMEND THE TOPSOIL, RE-TEST AND RESUBMIT THE ANALYSIS FOR APPROVAL PRIOR TO BRINGING THE TOPSOIL ON SITE.
- EXISTING TOPSOIL FROM ON SITE THAT HAS BEEN STOCKPILED MAY BE USED IF IT MEETS THE REQUIREMENTS FOR IMPORTED TOPSOIL. THIS INCLUDES CONDUCTING SOIL TEST ANALYSIS AND ADDING AMENDMENTS AS NEEDED SO THAT THE SOIL COMPLIES WITH THE REQUIREMENTS.
- SOD SHALL BE A TALL FESCUE BLEND SOD, SUCH AS BLACK BEAUTY OR APPROVED EQUAL. BLUEGRASS BLEND SOD IS NOT ACCEPTABLE, UNLESS APPROVED BY THE LANDSCAPE ARCHITECT. SODDED LAWN WILL NOT BE ACCEPTED UNTIL WELL ROOTED AND MOWN TWICE.
- SEEDED BIOSWALE MIX SHALL BE DETENTION BASIN MIX BY PRAIRIE MOON NURSERY APPLIED AT 9.5 LBS/ACRE. TEMPORARY COVER CROP TO BE 'REGREEN' WHEAT GRAIN HYBRID BY PRAIRIE MOON NURSERY, APPLIED AT 10 LBS/ACRE.
- ALL PLANT MATERIAL MUST BE SPECIMEN QUALITY, TRUE TO SPECIES AND VARIETY, WITH FULL, DENSE AND HEALTHY FORMS. PLANTS THAT ARE NOT SPECIMEN QUALITY WILL NOT BE ACCEPTED. ALL PLANTS MUST HAVE BEEN LOCALLY GROWN AT A STATE INSPECTED AND CERTIFIED NURSERY, LOCATED WITHIN 50 MILES OF THE PROJECT SITE.
- ALL DECIDUOUS AND EVERGREEN TREES AND SHRUBS THAT ARE 36" HEIGHT OR TALLER ARE TO HAVE FRESHLY DUG, BALLED AND BURLAPPED ROOT BALLS WITH SIZES THAT MEET THE STANDARDS OF THE AMERICAN NURSERYMEN'S ASSOCIATION. DECIDUOUS AND EVERGREEN SHRUBS THAT ARE 30" HEIGHT OR LESS MAY EITHER HAVE FRESHLY DUG, BALLED AND BURLAPPED ROOT BALLS OR MAY BE CONTAINER GROWN. ALL PERENNIALS, ORNAMENTAL GRASSES AND GROUNDCOVERS MUST BE CONTAINER GROWN. CONTAINER GROWN MATERIALS SHALL HAVE BEEN GROWN IN THEIR CURRENT CONTAINER FOR A PERIOD OF NO LESS THAN SIX MONTHS.
- ALL TREES SHALL BE INSPECTED BY THE LANDSCAPE ARCHITECT, AT THE NURSERY, PRIOR TO DIGGING. TREES THAT ARE ACCEPTABLE WILL BE SEALED. ALL SEALS APPLIED BY THE LANDSCAPE ARCHITECT MUST REMAIN ON THE TREES AND ARE NOT TO BE REMOVED BY THE CONTRACTOR. ALL TREES MUST HAVE SEALS IN PLACE AT TIME OF ACCEPTANCE OR WILL BE AUTOMATICALLY REJECTED. HEELED-IN TREES WILL NOT BE ACCEPTED WITHOUT PRIOR APPROVAL FROM THE LANDSCAPE ARCHITECT.
- TREES SPECIES SUCH AS BETULA, CELTIS, CERCIS, CRATAEGUS, FAGUS, LIRIODENDRON, PLATANUS, POPULUS, PRUNUS, PYRUS, QUERCUS AND TAXODIUM ARE CONSIDERED 'SPRING DIG ONLY' TREES. THE CONTRACTOR MUST HAVE THESE TREES ORDERED, ACCEPTED AND DUG IN EARLY SPRING, FOR SUMMER AND FALL LANDSCAPE INSTALLATIONS.
- ALL SHRUBS 36" HEIGHT OR TALLER ARE TO BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION. AN EXAMPLE SHRUB MAY BE TAGGED OR PICTURES MAY BE SUBMITTED FOR APPROVAL. ALL SHRUBS THAT DO NOT MATCH THE APPROVED EXAMPLE SHRUB IN SIZE AND FORM WILL BE REJECTED. ALL OTHER SHRUBS MUST MEET THE MINIMUM SIZES GIVEN IN THE PLANT LIST AND BE SPECIMEN QUALITY. SHRUBS THAT DO NOT MEET THESE GUIDELINES WILL BE REJECTED.
- PLANT MATERIAL SHALL ONLY BE INSTALLED WITHIN THE FOLLOWING DATES:
 DECIDUOUS TREES AND SHRUBS: APR 1 THROUGH NOV 15
 SPRING DIG DECIDUOUS TREES: APR 1 THROUGH OCT 15
 EVERGREEN TREES AND SHRUBS: APR 1 THROUGH NOV 1
 PERENNIALS AND GROUNDCOVERS: MAY 1 THROUGH OCT 15.
- TEST TREE PITS AND PLANT BEDS FOR ADEQUATE DRAINAGE. FILL PLANT PIT WITH 12" OF WATER. IF WATER LEVEL DOES NOT DROP A MINIMUM OF ONE INCH PER HOUR, NOTIFY THE LANDSCAPE ARCHITECT OF DRAINAGE ISSUES.
- PLANTING MIX FOR ALL PLANTINGS SHALL BE AS FOLLOWS. MIXING OF PLANTING MIX COMPONENTS SHALL BE THOROUGHLY BLENDED OUTSIDE THE PLANTING PITS, PRIOR TO PLANTING.
 TREES AND SHRUBS: 80% TOPSOIL, 10% SAND, 10% FINE FINES
 PERENNIALS AND ANNUALS: 10% TOPSOIL, 15% SAND, 15% FINE FINES
- FOR PLANTING MIXES, SAND SHALL BE ANGULAR, COARSE SAND AND FINE FINES SHALL BE 3/8" SOUTHERN PINE BARK FINES.
- EXCAVATE PARKING LOT ISLANDS AND CURBED PLANTERS TO A DEPTH OF 24" AND BACKFILL WITH TOPSOIL. BERM PLANTERS AND ISLANDS TO A HEIGHT OF 6" ABOVE TOP OF CURB.
- ALL PLANT BEDS ARE TO BE WELL SHAPED 'SPADE CUT' EDGES, 3" DEPTH, IN LINES OR IN CURVES AS SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL REQUEST AN INSPECTION OF LANDSCAPE PLANTING UPON SUBSTANTIAL COMPLETION FOR "ACCEPTANCE". REQUEST MUST BE SUBMITTED TO LANDSCAPE ARCHITECT AT LEAST TEN DAYS PRIOR TO ANTICIPATED INSPECTION. UPON INSPECTION THE LANDSCAPE ARCHITECT SHALL PREPARE A PUNCH LIST OF UNACCEPTABLE OR OUTSTANDING ITEMS. AFTER THE ITEMS HAVE BEEN SATISFACTORILY COMPLETED AND REVIEWED, THE LANDSCAPE ARCHITECT WILL RECOMMEND "ACCEPTANCE".
- ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR FROM DATE OF "ACCEPTANCE". AT OR NEAR THE END OF THE GUARANTEE PERIOD, THE CONTRACTOR SHALL REQUEST AN INSPECTION OF LANDSCAPE PLANTING FOR "FINAL ACCEPTANCE". REQUEST MUST BE SUBMITTED TO LANDSCAPE ARCHITECT AT LEAST TEN DAYS PRIOR TO ANTICIPATED INSPECTION. UPON INSPECTION THE LANDSCAPE ARCHITECT SHALL PREPARE A PUNCH LIST OF UNACCEPTABLE ITEMS. AFTER THE ITEMS HAVE BEEN SATISFACTORILY REPLACED OR REPAIRED, REVIEWED AND APPROVED, THE LANDSCAPE ARCHITECT WILL RECOMMEND "FINAL ACCEPTANCE".

IRRIGATION NOTES

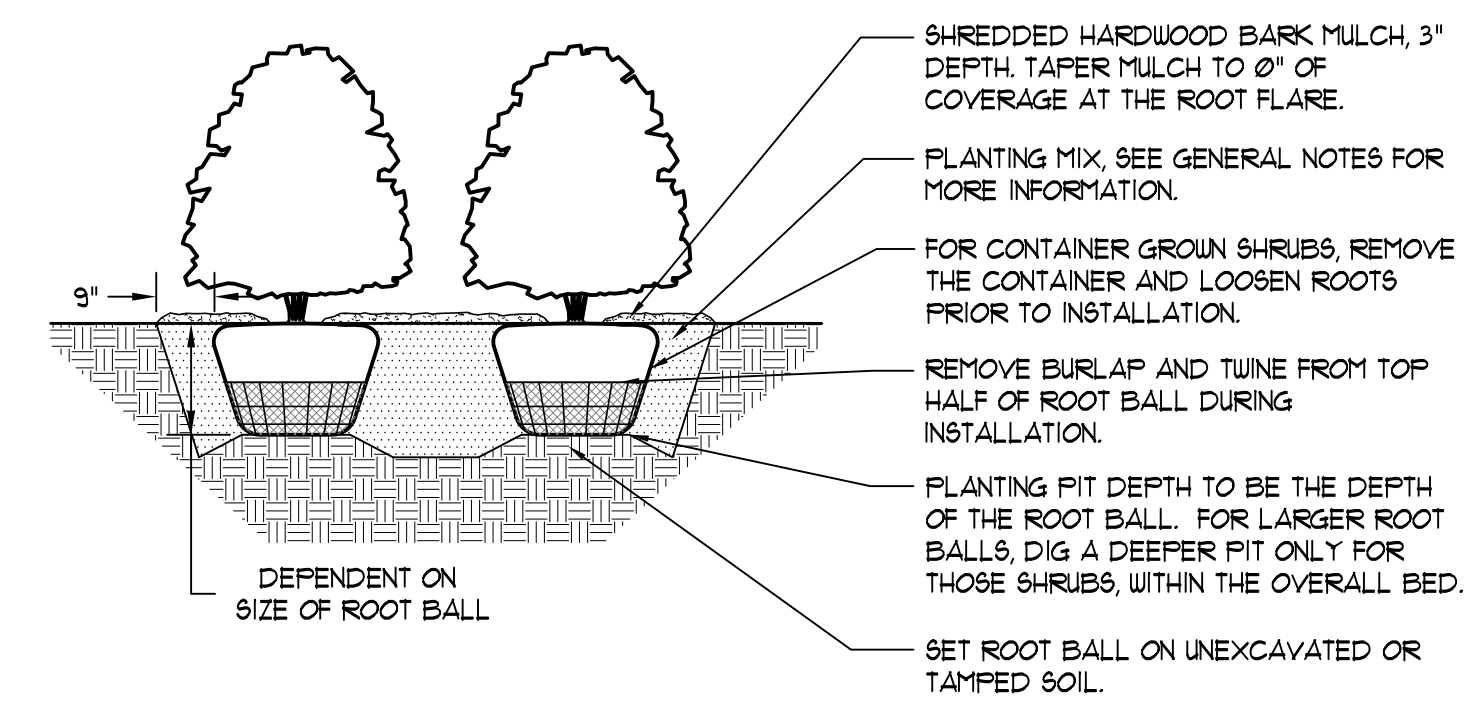
- A PLASTIC, AUTOMATIC, TIMER-ACTIVATED IRRIGATION SYSTEM SHALL BE PROVIDED FOR ALL LANDSCAPE AREAS. SEPARATE ZONES TO BE PROVIDED FOR LAWN AND PLANTED BEDS.
- PROVIDE ROTARY AND MIST POP-UP SPRAYHEADS AS APPROPRIATE FOR THE PARTICULAR SIZE AND SHAPE OF LAWN OR PLANTING AREA, WITH 60% HEAD TO HEAD SPACING. THERE SHALL BE NO OVER SPRAY ON ROADS OR SIDEWALKS.
- PROVIDE DRIP IRRIGATION WHERE NOTED ON THE PLANS. DRIP IRRIGATION TO PROVIDE PROPER MOISTURE LEVEL FOR SPECIFIED PLANTS (SEE PLANS). A DRIP SYSTEM OPERATION INDICATOR SHALL BE PROVIDED FOR EACH ZONE.
- PROVIDE WATER CONNECTION, BACKFLOW PREVENTER AND IRRIGATION CONTROLLER INSIDE THE BUILDING.
- SIZE BOOSTER PUMP, IF REQUIRED, TO PROVIDE 50 GPM AND 40 PSI AT THE SPRINKLER HEADS.
- PROVIDE SLEEVEING AS NECESSARY AND COORDINATE SLEEVE INSTALLATION WITH THE GENERAL CONTRACTOR TO ELIMINATE FUTURE DAMAGE TO INSTALLED PAVEMENT.
- ALL IRRIGATION EQUIPMENT SHALL BE BY TORO, RAINBIRD, OR APPROVED EQUAL.
- THE IRRIGATION CONTRACTOR SHALL SUBMIT SHOP DRAWINGS SHOWING THE DESIGN OF THE SYSTEM FOR APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION. THE DRAWING SHOULD INDICATE THE WATER CONNECTION, BACKFLOW PREVENTER, PIPING, INCLUDING SIZES, VALVES, INCLUDING SIZES, HEADS, INCLUDING LOCATIONS, TYPE AND INDICATION OF SPRAY RADIUS, DRIFTLINES, INCLUDING EMITTERS, QUICK COUPLERS, THE AUTOMATIC RAIN SHUTOFF AND THE PROPOSED ELECTRICAL CONNECTION. PROVIDE CATALOGUE CUTS FOR ALL EQUIPMENT.
- PROVIDE AN AUTOMATIC RAIN SHUT-OFF.
- PROVIDE A QUICK COUPLER IN OR ADJACENT TO EACH VALVE BOX, AND AT INTERVALS ALONG THE MAIN WITHIN 100' OF ALL NEW PLANTS, OR AS INDICATED ON THE PLANS.
- THE IRRIGATION CONTRACTOR SHALL BE RESPONSIBLE FOR BLOWING OUT THE SYSTEM BY AIR COMPRESSION IN THE FALL FOLLOWING INSTALLATION AND TURNING ON THE SYSTEM IN THE FOLLOWING SPRING.
- THE ENTIRE IRRIGATION SYSTEM SHALL BE GUARANTEED BY THE CONTRACTOR AS TO MATERIAL AND WORKMANSHIP, INCLUDING SETTLING OF BACKFILLED AREAS BELOW GRADE FOR A PERIOD OF ONE YEAR FOLLOWING THE DATE OF FINAL ACCEPTANCE OF THE WORK.
- ALL WORK SHALL BE IN ACCORDANCE WITH LOCAL MUNICIPALITY STANDARDS, CODES AND ORDINANCES.

LOCATE ALL UNDERGROUND UTILITIES PRIOR TO DIGGING. TREE PIT WIDTH TO BE THREE TIMES THE WIDTH OF THE ROOT BALL. TEST TREE PIT FOR DRAINAGE PRIOR TO INSTALLING TREE. PRUNE OFF ALL DEAD, BROKEN OR SCARRED BRANCHES, AND SHAPE PRUNE AS DIRECTED BY THE LANDSCAPE ARCHITECT. FOR TREES UP TO 4" CALIPER OR 12' HEIGHT, REMOVE APPROXIMATELY 5% -10% OF THE OVERALL BRANCHING. FOR TREES OVER 4" CALIPER OR 12' HEIGHT, REMOVE APPROXIMATELY 15%-20% OF THE OVERALL BRANCHING. LOCATE ROOT FLARE IN ROOT BALL AND SET TREE HEIGHT SO THAT ROOT FLARE IS FLUSH WITH FINISH GRADE. REMOVE ALL BASKET WIRE, TWINE AND BURLAP FROM THE TOP HALF OF THE ROOT BALL. WATER IN THE PLANTING MIX THOROUGHLY, WHILE KEEPING THE TREE PLUMB. STRAIGHTEN TREE IF SETTLING OCCURS.



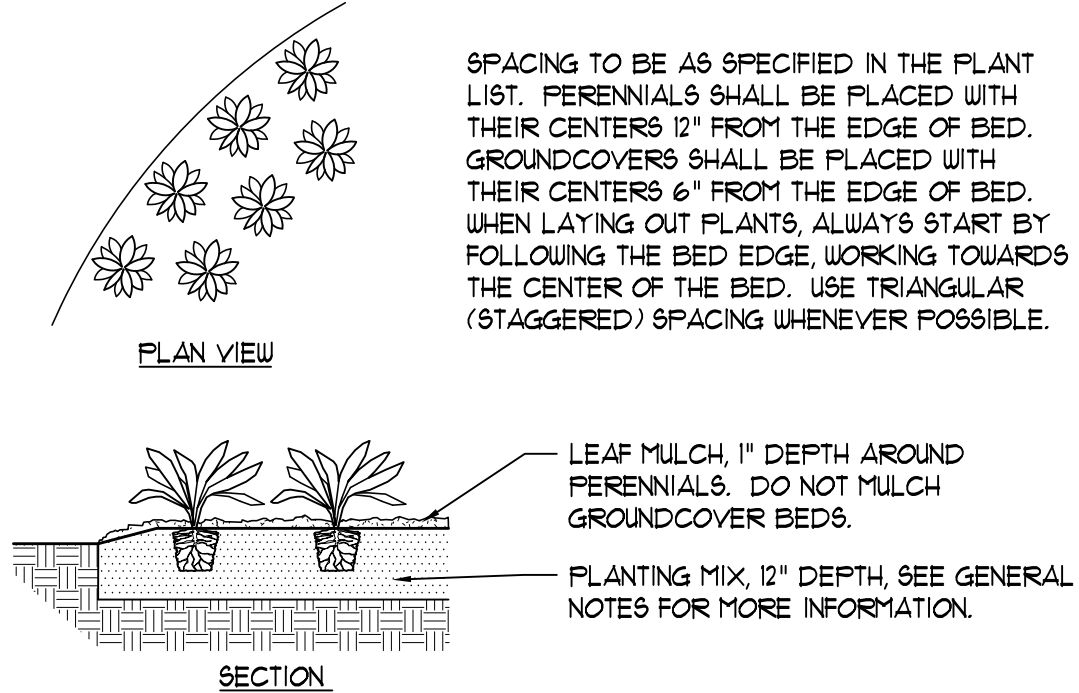
1 DECIDUOUS TREE PLANTING DETAIL
NOT TO SCALE

LOCATE ALL UNDERGROUND UTILITIES PRIOR TO DIGGING. SHRUB PIT WIDTH TO BE EXTENDED 9" AROUND THE ENTIRE ROOT BALL, AS SHOWN IN THE DETAIL BELOW. TEST SHRUB PIT FOR DRAINAGE PRIOR TO INSTALLING SHRUB. PRUNE OFF ALL DEAD, BROKEN OR SCARRED BRANCHES, AND SHAPE PRUNE AS DIRECTED BY THE LANDSCAPE ARCHITECT. LOCATE ROOT FLARE IN ROOT BALL AND SET SHRUB HEIGHT SO THAT ROOT FLARE IS FLUSH WITH FINISH GRADE. REMOVE ALL TWINE FROM AROUND THE ROOT FLARE. WATER IN THE PLANTING MIX THOROUGHLY, WHILE KEEPING THE SHRUB PLUMB. STRAIGHTEN SHRUB IF SETTLING OCCURS. MULCH LIMITS FOR SHRUBS TO EXTEND TO ALL EDGES OF PLANTING BEDS; SEE PLANS FOR BED LAYOUTS.



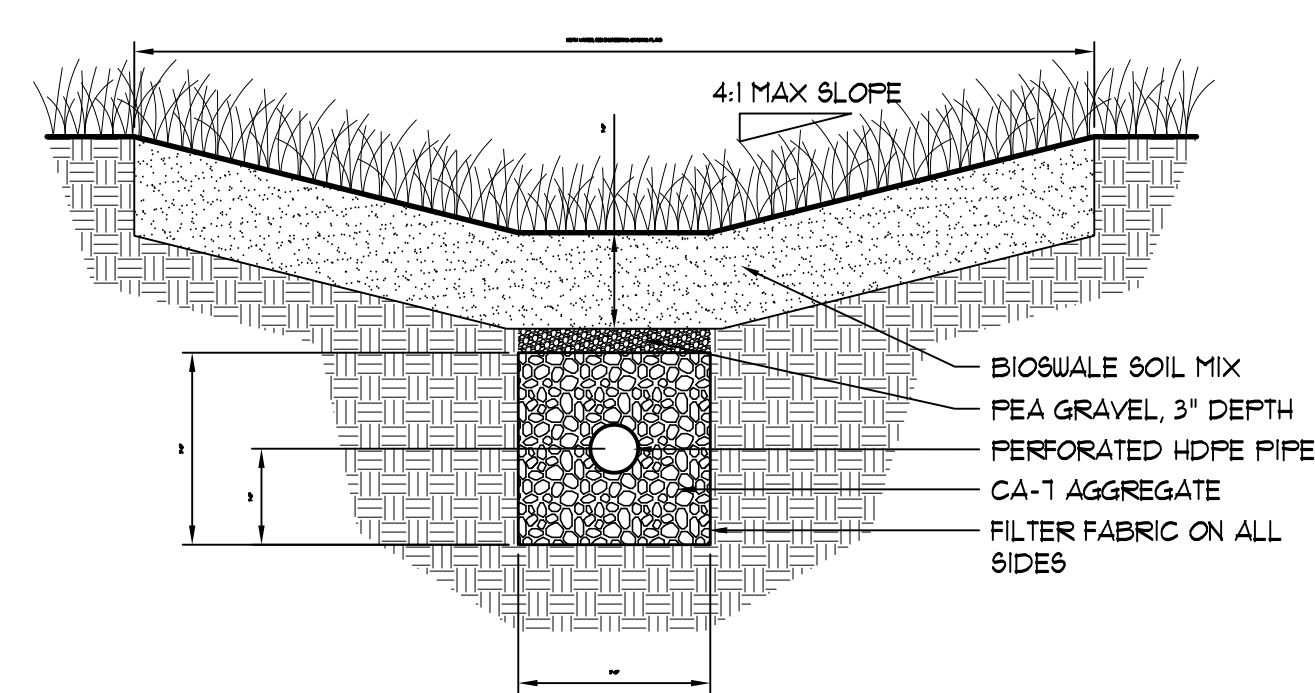
2 SHRUB PLANTING DETAIL
NOT TO SCALE

LOCATE ALL UNDERGROUND UTILITIES PRIOR TO DIGGING. EXCAVATE ENTIRE PERENNIAL / GROUNDCOVER BED, AND BACKFILL WITH PLANTING MIX AS SPECIFIED. BED HEIGHT IS TO BE 2" ABOVE FINISH GRADE AND WELL DRAINED. MULCH LIMITS FOR PERENNIAL BEDS TO EXTEND TO ALL EDGES OF THE BEDS; SEE PLANS FOR BED LAYOUTS. SEE PLANT LIST FOR PLANT SPACING. GAPS BETWEEN PLANTS SHALL BE NO GREATER THAN THE SPECIFIED SPACING FOR THAT PARTICULAR PLANT.

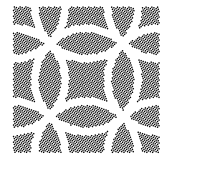


3 PERENNIAL AND GROUNDCOVER PLANTING DETAIL
NOT TO SCALE

SEE ENGINEERING DRAWINGS FOR INFORMATION ON GRADING, SLOPES, PIPE DIAMETER AND WIDTH OF BIOSWALE. CONNECT OR DAYLIGHT PIPE AS INDICATED ON THE ENGINEERING DRAWINGS. FILTER FABRIC SHALL BE A NON-WOVEN, NEEDLE PUNCHED GEO-TEXTILE FABRIC, BETWEEN 3 AND 45 OZ. THE WIDTH OF THE BIOSWALE SHALL BE COVERED WITH AN EROSION CONTROL FABRIC AFTER SEEDING. SEE LANDSCAPE PLAN AND SEEDING NOTES FOR SEED INFORMATION.



4 TYPICAL BIOSWALE
NOT TO SCALE



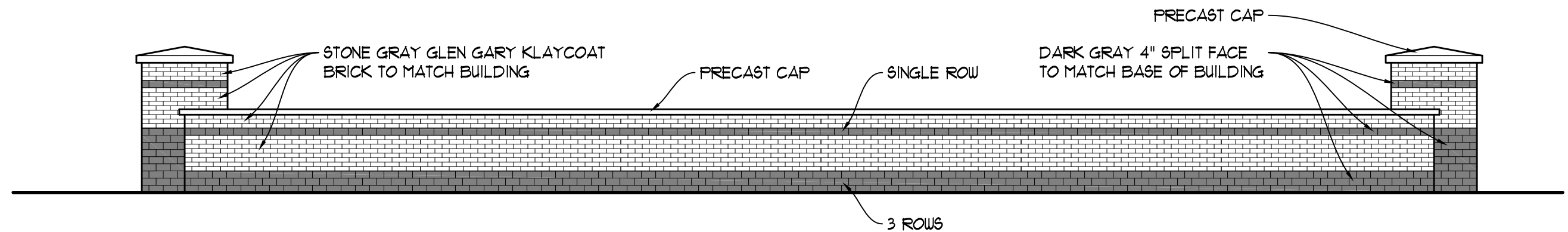
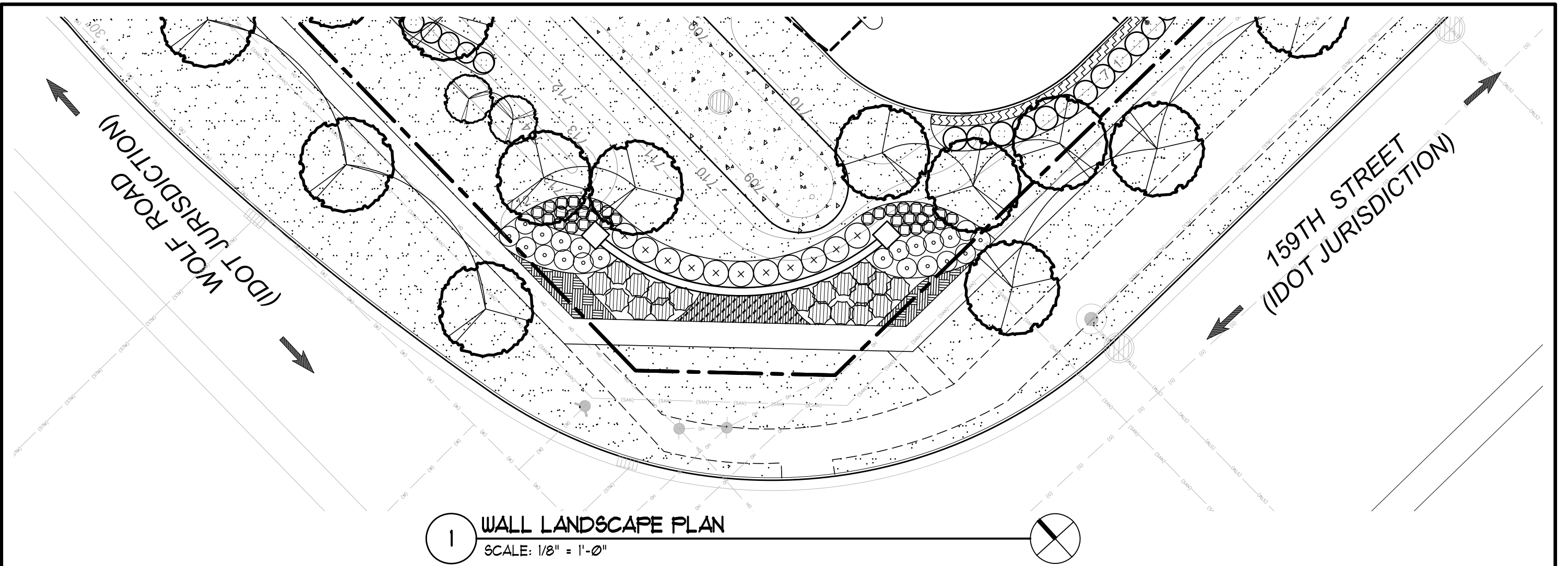
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Project
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159TH ST & WOLF RD
ORLAND PARK, ILLINOIS

Sheet Title	
LANDSCAPE DETAILS	
Date 08/31/18	Project No. DWP 18-179
Scale 1" = 20'-0"	Sheet No. L-3.0
Drawn By RT	
Approved WS	



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TOMMY'S CAR WASH
ORLAND PARK

WALL ELEVATION
SCALE: AS NOTED

LSK-1

2019-02-08



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†Also admitted in Wisconsin

DAVID B. SOSIN
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RESPONSES TO SPECIAL USE REQUIREMENTS

The applicant proposes to construct a state of the art tunnel car wash on the Northeast corner of 159th Street and Wolf Road. The applicant is seeking a Special Use Permit as required by the Land Development Code with modifications for the following: (a) A modification to locate drive-thru lanes and a n the setback area between the building façade and the street (Section 6-207.F.4); b) A modification to increase the number of drive thru lanes from 1 lane with a maximum width of 14 feet to 3 lanes with a combined width to of 36.5 feet (Section 6-207.F.4); c) A modification to increase the number of parking spaces from 2 as required to 22 (Section 6-306.B; and d)A modification to permit drive through land to the car wash tunnel of more one lane between a building and a street, of more than the maximum fourteen feet and with a turning radius of less than twenty feet. (Section 6-207.F.4 a: a)

In support of its request and in response to the Village's special use requirements, the applicant responds as follows:

1. The special use will be consistent with the purposes, goals, objectives and standards of the Comprehensive Plan, any adopted overlay plan and these regulations.

➤ *Response.* The site was originally proposed to be developed with a retail center. The current zoning would permit such a use, but the trend of on-line sales and glut of vacant commercial space creates a need to look to other uses for the property that are both feasible and commercially sustainable. The proposed use is complementary to the adjoining BMW auto dealership and well positioned for the increased traffic that will like be generated on 159th Street and Wolf Road improvements. All of the variances requested are necessary and the plans are customary for a successful and well-planned car wash operation.

The car wash use is an ideal use of a formally envisioned retail center, which would be economically challenged. The lane configuration and multiple stacking lanes are necessary for the proper and efficient operation of the car wash.

2. The special use will be consistent with the community character of the immediate vicinity of the parcel proposed for development.

- *Response.* While there is a residential development to the North, that development is a substantial distance away and buffered by an adjoining commercial parcel to the North. Petitioner believes that the property takes its character from the adjoining BMW Sales and Service facility, the adjacent commercial and banking centers on the three adjoining corners of the intersection. Because the requested car wash use is a Special Use, it is contemplated as a possible feasible use of the property under the Land Development Code

3. The design of the proposed use will minimize adverse effects, including visual impacts on adjacent properties.

- *Response.* The Petitioner has worked with staff to implement the landscaping requirements for the property and provide additional landscaping on the West and North perimeters of the property. Landscaping as proposed will exceed the requirements of the Code, notwithstanding the commercial character of the Wolf Road and 159th Street intersection. The developer is employing the innovation in lighting technology to bring maximum appeal to the site, with the smallest spillover of lighting at less than the required 5.0 foot candles at the lot line.

A traffic study confirmed the feasibility of the use.

4. The proposed use will not have an adverse effect on the value of adjacent property.

- *Response.* The long period of vacancy and failed plan of retail development of the property has been a hindrance to the adjoining business community. Petitioner believes that the increased activity from the car wash will bring new customers and traffic to the center to the West, the Southwest, and the bank to the South. It is contemplated that future development on the lots adjacent on the North and East of the car wash will also have a positive effect on developing the corner of Wolf Road and 159th Street as a vibrant business center as envisioned in the long range plan and the BIZ zoning of the property.

The Petitioner has conducted a traffic study which determined the use to be feasible.

The traffic study further determined that the existing condition of traffic at 159th Street and Wolf Road would not be adversely impacted by the proposed development.

5. The applicant has demonstrated that public facilities and services, including but not limited to roadways, park facilities, police and fire protection, hospital and medical services, drainage systems, refuse disposal, water and sewers, and schools will be capable of serving the special use at an adequate level of service.

- *Response.* Because the proposal is an implementation of a previous plan of development all roadway and infrastructure improvements to the general area have

previously been installed. The Petitioner has submitted preliminary engineering that will be approved prior to consideration by the Village Plan Commission. The impact of development on roadways and parks, have been previously reviewed and approved by the Board of Trustees in consideration of the prior Petition for Development approving the overall 159th Street and Wolf Road site.

See #4 above.

6. The applicant has made adequate legal provision to guarantee the provision and development of any open space and other improvements associated with the proposed development.

➤ Response. See response to Standard 5. As required by the zoning code, the plan for Tommie's Car Wash provides for lot coverage not to exceed 75% of the property. The landscaping, pavement and numerous amenities and enhancements more than compensate for any deviation for the location of the driveway adjacent to the street.

7. The development will not adversely affect a known archaeological, historical or cultural resource.

➤ Response. The applicant is not aware of any archaeological, historical or cultural resources which will be adversely affected by the use of the site as a commercial use. The applicant is not aware of any archeological, historical or cultural resources that would be destroyed by the proposed development.

8. The proposed use will comply with all additional standards imposed on it by the particular provision of these regulations authorizing such use and by all other requirements of the ordinances of the Village.

➤ Response. Except as noted in the introductory paragraph, Petitioner believes it can fully comply will all additional standards contained in the Orland Park Development Code.

Justification for the requested modifications

- a) A modification to locate drive-thru lanes and a parking lot in the setback area between the building façade and the street (Section 6-207.F.4);

Petitioner's Justification. The Land Development Code discourages the placement of driveways adjacent to streets without mitigating design features. The Petitioner's architects have extensive experience in the design of the Tommy's Car Wash facilities across the country. Specifically, the flow of traffic through these washes is vital to move vehicles on and off of the facility in a time frame that is far less than a traditional car wash design. The Petitioner and its team have determined that it is vital to the proper operation of the facility that the drive-thru lanes be located adjacent to Wolf Road and that the entrance to the wash be located adjacent to 159th Street. To mitigate any negative aesthetic conditions created by the design the Petitioner has proposed a substantial landscape Berm adjacent to the drive-thru lanes and significant landscaping to be located on the Berm.

- b) A modification to increase the number of drive thru lanes between the building façade and the street from one lane with a maximum of fourteen (14') feet to three (3) lanes with a combined width of thirty six and one-half (36.5') feet (Section 6-207.F.4);

Petitioner's Justification. The architects and engineers for the car wash have studied the stacking and flow of Tommy's Car Wash designs all over the country. While the Petitioner believes that the stacking of cars exceeds any required stacking requirements, the Petitioner understands the concerns of the Staff of the Village. To accommodate the concern of staff, the plan has maximized the available stacking to insure that no cue of vehicles ever interferes with the traffic flow on Wolf Road.

- c) A modification to increase the number of parking spaces from 2 as required to 22 (Section 6-306.B; and

Petitioner's Justification. While only two spaces are required the design of wash includes self-service vacuum areas. The design of a Tommy's car wash includes the maximization of vacuum units to assure there is no waiting or bottleneck on the site. While the Land Development Code does not differentiate between parking spaces and spaces for vacuuming, it is these spaces that results in need for a modification as if the site is over-parked.

Respectfully submitted,

By: 
David B. Sosin
Attorney for Petitioner

DATE: February 19, 2019

REQUEST FOR ACTION REPORT

File Number:	2019-0003
Orig. Department:	Development Services Department
File Name:	Solar Panel Installation - 88 Silo Ridge Road West

BACKGROUND:

The petitioner withdrew the petition on April 12, 2019, prior to the scheduled Plan Commission meeting on April 16, 2019.

BUDGET IMPACT:

REQUESTED ACTION:

None

DATE: February 19, 2019

REQUEST FOR ACTION REPORT

File Number: **2019-0142**
Orig. Department: **Development Services Department**
File Name: **Memo: New Petitions & Appearance Review**

BACKGROUND:

BUDGET IMPACT:

REQUESTED ACTION:



Memorandum

To: Plan Commission
From: Ed Lelo, Planning Manager of Development Services
Date: April 16, 2019
Subject: New Petitions & Appearance Reviews

Below, please find a summary of recently petitioned projects and appearance reviews. Petitioned projects are currently under review by staff and may or may not be on a future Plan Commission agenda. These projects have been petitioned to the Village but may not have obtained all the approvals required to begin work. Projects sometimes are terminated without moving forward for a variety of reasons. Appearance Reviews and Certificates of Appropriateness are reviewed and approved administratively. The below list does not include cell tower colocation projects. Please contact me with any questions regarding the below projects.

Appearance Review Petitions

Trader Joe's – 14924 LaGrange Rd
Fence

Development Petitions

Chick Fil A – 15605 LaGrange Rd.

Panera Bread – 7420 159th St.

Board Approved Petitions

Appearance Improvement Grant

Certificate of Appropriateness Petition

Wilson Residence – 9967 144th St.
Retaining wall replacement



AIRLIFT DOORS, INC.

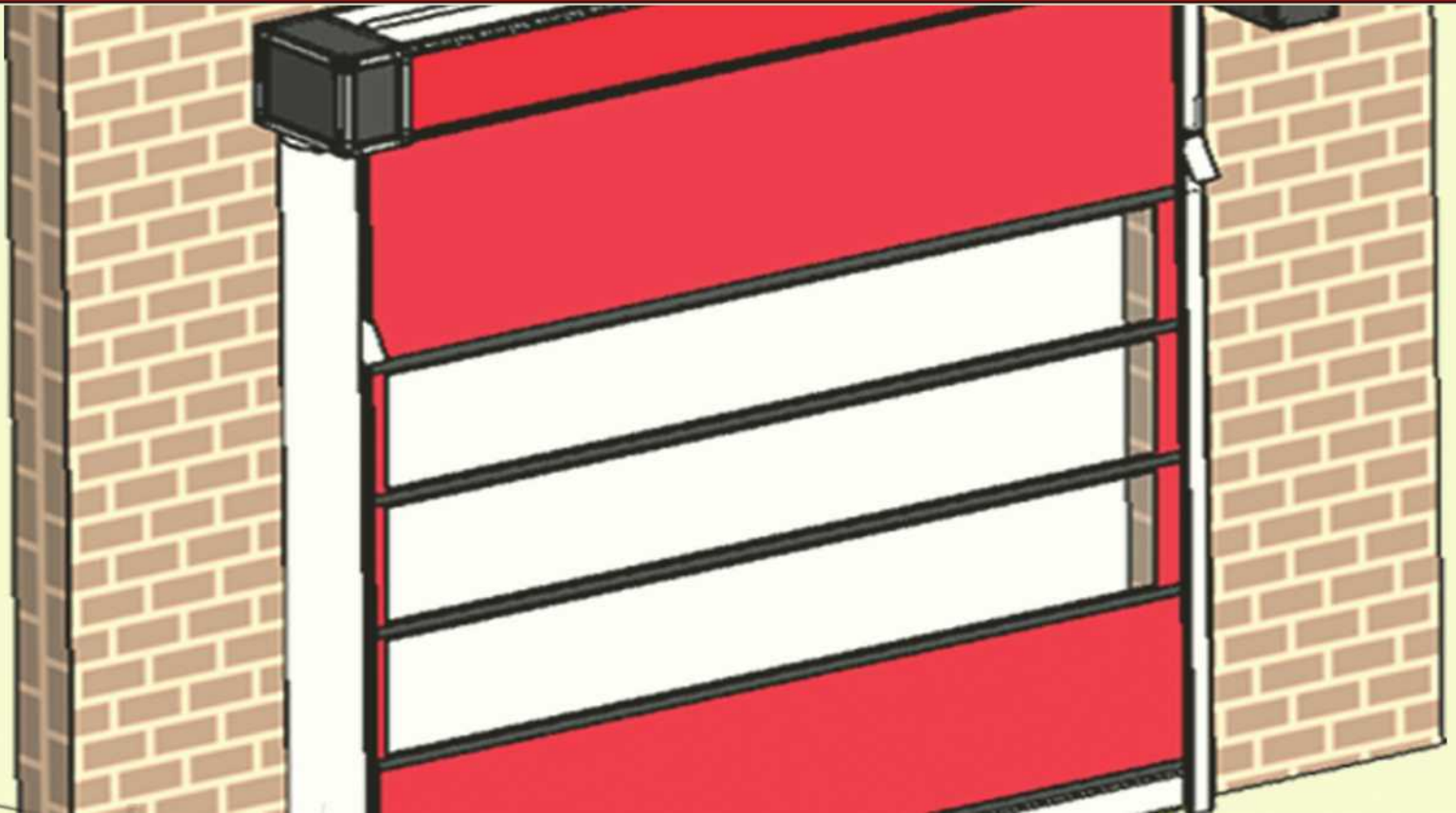
Owner's Manual

Premium & Elite Models

XRS Roll Up Door

www.AirliftDoors.com

1-888-368-4403





WARNING

The material provided in this owner's manual is meant for reference purposes only. Only persons trained and qualified to work on Airlift Doors, Inc. products should perform maintenance and service on any products supplied by Airlift Doors, Inc.

This owner's manual only provides partial information regarding our products. Further information such as installation guides, wiring diagrams, and other technical information should be referenced to ensure safe and proper installation and maintenance of Airlift Doors, Inc. products.

Always consult a trained professional installer or service provider to properly service your equipment. Garage doors and garage door openers are extremely dangerous and improper handling of these products may result in serious injury or death if handled improperly.

Always ensure that all power is disconnected to components before you perform any service to any Airlift control box or motor.

Always shut off and drain any air supply associated with an Airlift operator before performing service on the component.

Never work on a door that is counterbalanced without referencing the installation guide or consulting the factory for proper methods to work on counterbalanced doors.

Never stand underneath a garage door to test any function of the door, opener, or safety devices.

Thank you for purchasing a XRS Vinyl Roll Up Door from Airlift Doors, Inc. We take great pride in the quality and workmanship of our products and stand behind our products 100%. If you have any questions or concerns please contact us directly and we would be glad to assist you in any way possible.

This owner's manual is meant to be an informational tool for you to operate and maintain your XRS Vinyl Roll Up Door. Further information is available on our website or by contacting us directly. Please reference the contact information below.

We greatly appreciate your business and promise to work hard to provide you with the highest level of service at all times.

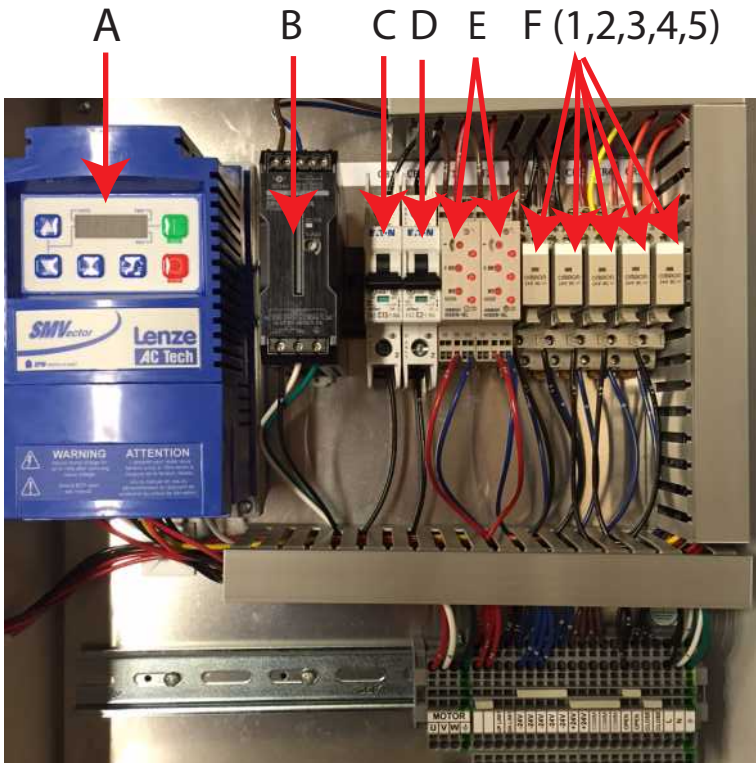
Thank you again for choosing Airlift Doors.

Airlift Doors, Inc.
1-888-368-4403
www.airliftdoors.com

Table of Contents

- I. Basic Operation of the XRS Roll Up Door**
- II. Speed Adjustments**
- III. Door Limit Adjustments**
- IV. Manual Door Operation**
- V. Door Section Replacement Instructions**
- VI. Strap Replacement Instructions (Premium Model Only)**
- VII. Limit Switch Replacement Instructions**
- VIII. Troubleshooting**
- IX. Parts Diagram**
- X. Wiring Diagrams**
- XI. Maintenance**
- XII. Limited Warranty**
- XIII. Returned Goods Authorization**

XRS Plus Model VFD Control Component Descriptions



- A: R08R5100 - XRS Plus VFD/Inverter Drive
- B: R08R5103 - XRS Plus 24VDC Power Supply
- C: R08R5101 - XRS Plus 13 Amp Circuit Breaker
- D: R08R5102 - XRS Plus 2 Amp Circuit Breaker
- E: R08R5104 - XRS Plus Limit Interrupt Timer (2)
- F: R08R5105 - XRS Plus 24VDC Signal Relay (5)

A: The VFD/Inverter drive is the brains of the control. It takes 120VAC power and converts it to 3 Phase / 230VAC power to be used by the motor. It also allows us to vary the electrical frequency which is how we change the speed of the door.

B: The 24VDC power supply takes the 120VAC incoming power and converts it to 24VDC to be used by all accessories, timers, relays, etc. inside the control.

C: The 13 amp circuit breaker provides electrical over-current safety for the incoming 120VAC power. This basically protects the 24VDC power supply and the VFD/Inverter drive.

D: The 2 amp circuit breaker protects all 24V accessories, timers, & relays.

E: The Limit Interrupt Timers interrupt the STOP signal that is sent to the VFD when either the Open or Close limit is reached. They turn off this stop signal so the door can be operated in the opposite direction. Without these, when a limit is reached, the stop signal would remain on constantly. The left hand timer is for the open limit and the right hand timer is for the close.

F1: This 24VDC Signal Relay turns on when the Open limit is pressed in. It's purpose is to prevent the door from traveling past the open limit if an open signal is received when the door is open.

F2: This 24VDC Signal Relay turns on when the Closed limit is pressed in. It's purpose is to prevent the door from traveling past the close limit if a close signal is received when the door is closed.

F3: This 24VDC Signal Relay turns on when a open signal is received. It sends the open signal to the VFD.

F4: This 24VDC Signal Relay turns on when a close signal is received. It sends the close signal to the VFD.

F5: This 24VDC Signal Relay turns on when any open signal is present. It prevents a close signal from overriding an open signal.

XRS Plus - Normal Operation Explanation

Door Opening

1. When an open signal is sent to the control from any source, the following happens:
 - a. Signal relay #3 is energized, which causes the VFD to run.
 - b. Signal relay #5, on the lower DIN rail, energizes. This allows an open signal to override any existing close signal.
2. As the door opens, the limit cam switches rotate clockwise.
3. Once the open limit cam (Inner Green) reaches the limit switch, the following happens:
 - a. The left hand Limit Interrupt Timer energizes and sends a Stop signal to the VFD.
 - b. There are 2 lights on the timer, orange and green. The orange light turns on for 1 second when the stop signal is being sent. The green light remains on until the door closes.
 - c. Signal relay #1 energizes when the open limit is pressed in. This prevents the door from opening past the open limit. To verify this, press the open button when signal relay #1 is energized and notice that signal relay #3 does not energize. Relay #1 will remain energized until the door closes.

Door Closes

1. When a close signal is sent to the control from any source signal relay #4 is energized, which causes the VFD to run.
2. As the door closes, the limit cam switches rotate counter-clockwise.
3. Once the close limit cam (Inner White) reaches the limit switch, the following happens:
 - a. The right hand Limit Interrupt Timer energizes and sends a Stop signal to the VFD.
 - b. There are 2 lights on the timer, orange and green. The orange light turns on for 1 second when the stop signal is being sent. The green light remains on until the door opens.

- c. Signal relay #2 energizes when the close limit is pressed in. This prevents the door from closing past the close limit. To verify this, press the close button when signal relay #2 is energized and notice that signal relay #4 does not energize. Relay #2 will remain energized until the door opens.

I. Normal Operation and Adjustments

A. Speed Adjustments

The speed of the door is controlled by adjusting the speed setting on the Variable Frequency Drive (VFD) located in the control box. See below.

****IMPORTANT**** ANY ADJUSTMENT OF THE DOOR SPEED AFTER THE INITIAL DOOR LIMITS HAVE BEEN SET WILL REQUIRE YOU TO RESET THE LIMITS !



UP arrow increases opener speed

Digital readout shows current opener speed as a percentage of the maximum speed. The range is from 0% to 100%. A setting of 100% is equal to approximately 30" per second of door travel.

DOWN arrow decreases opener speed

****NOTE**** The remaining buttons on the VFD are NOT USED.

OPEN AND CLOSE SPEED ARE NOT INDIVIDUALLY ADJUSTABLE

****CAUTION**** When adjusting the speed of the door, be prepared to press the STOP button on the front of the control box to prevent the door from rolling up or down too far.

B. Open and Close Limit Adjustments

****The opener limits are set by adjusting the Open and Close limit cams inside the gear head portion of the motor. Before proceeding, remove the black plastic cover from the opener so you have access to the Limit Assembly.**



Limit Set Screws

Each limit cam has two hex screws that can be used to adjust its position. These are a Set Screw and a Fine Tune Adjustment Screw.

****NOTE**** A small adjustment of the limit cam will result in a comparatively large change in door travel.

Loosening the Set Screw will completely loosen the cam from its shaft and allow you to rotate it freely. This screw should be used for large adjustments only.

For smaller adjustments (12" of door travel or less) use the Fine Tune Adjustment Screw. Turning this screw will rotate the cam as you turn it rather than completely loosen the cam.

Turning the Fine Tune Adjustment Screw clockwise will rotate the cam in the opposite direction (counterclockwise).

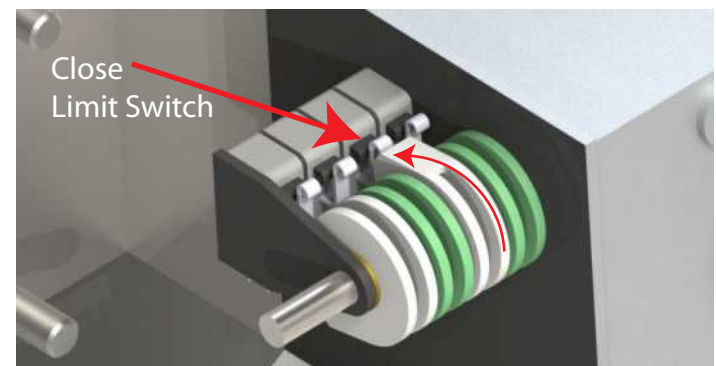
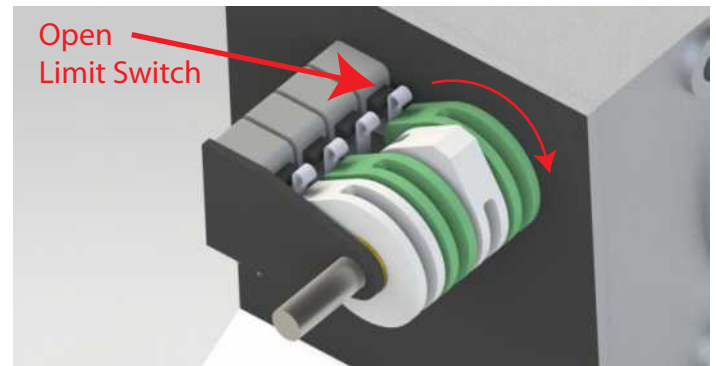
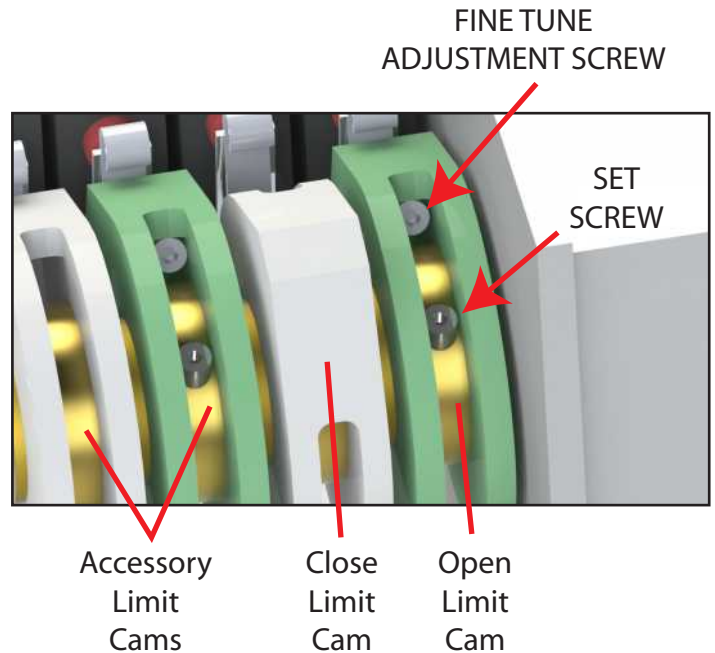
Likewise, turning the Fine Tune Adjustment Screw in the counter-clockwise direction will rotate the cam clockwise.

Open Limit Adjustment

During normal door travel, the Open Limit Cam rotates in the clockwise direction and contacts the limit switch from the bottom. To decrease the door travel, rotate the open limit cam (Green) slightly clockwise. To increase door travel, rotate the open limit cam slightly counterclockwise.

Close Limit Adjustment

During normal door travel, the Close Limit Cam rotates in the counter-clockwise direction and contacts the limit switch from the top. To decrease the door travel, rotate the close limit cam (White) slightly counter-clockwise. To increase door travel, rotate the close limit cam slightly clockwise.



****IMPORTANT****

Once the opener limits have been set, any adjustment to the opener speed will require that you adjust the opener limits as well.

Manual Door Operation

In the event of a power outage, the XRS door can be opened manually. How this is done depends on which XRS model you purchased. See below.

Elite Model XRS Door Manual Operation - (Non-counterweighted model)

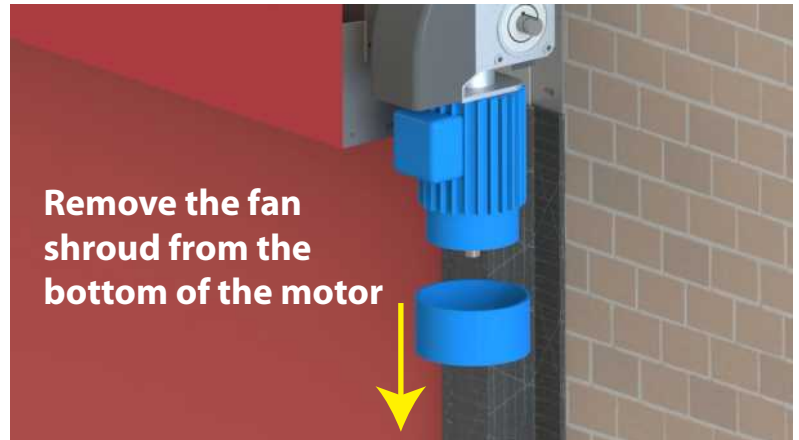
To open the Elite model door manually, you must use the manual motor socket on an electric drill to physically turn the motor shaft. See below.



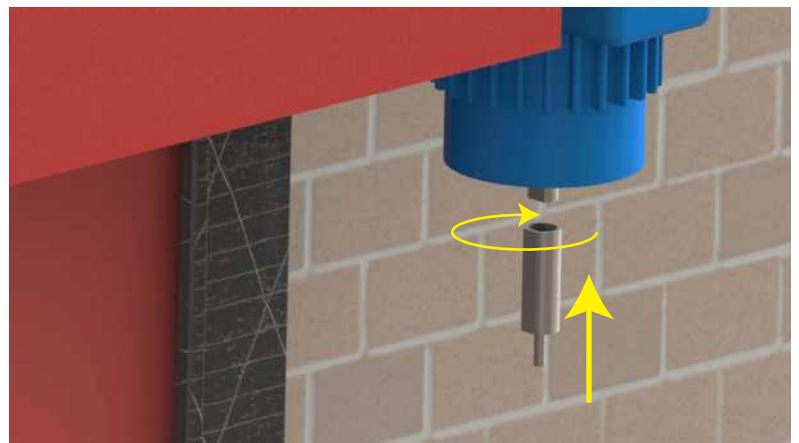
Manual Motor Socket

**Can be found on the inside cover of your VFD control box.

Slide the manual motor socket over the motor drive shaft and use a drill to spin the motor shaft. The door can be both opened and closed in this manner.



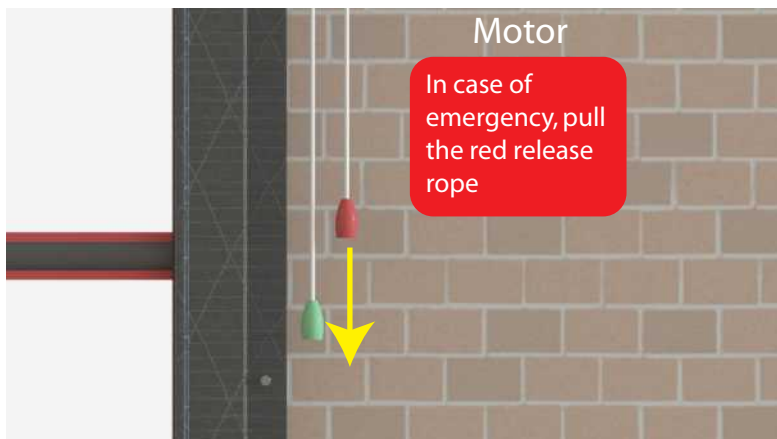
Remove the fan shroud from the bottom of the motor



Premium Model XRS Door Manual Operation - (With Strapeze[®] Counterweight System)

To open the Premium model door manually simply disengage the gear head by pulling down on the emergency release rope. This will allow the counterweights to open the door manually. To re-engage the gear head, pull the opposite rope and run the door with the Open/Close buttons.

**Failure to re-engage the gear head will prevent the door from running automatically.



Motor

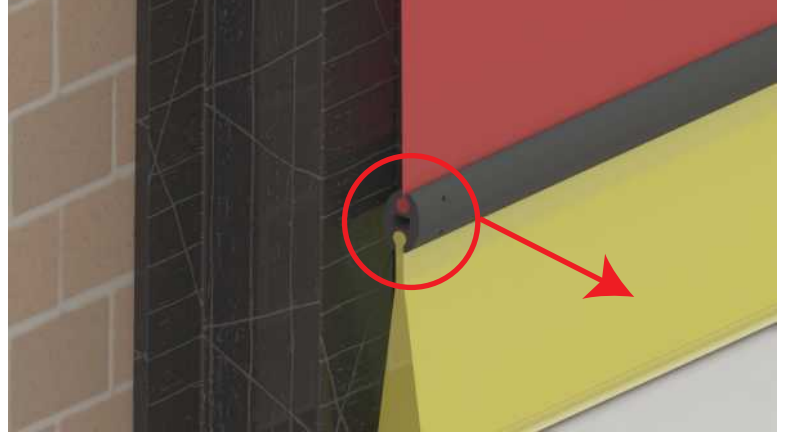
In case of emergency, pull the red release rope

To close the XRS Premium Model Door manually:

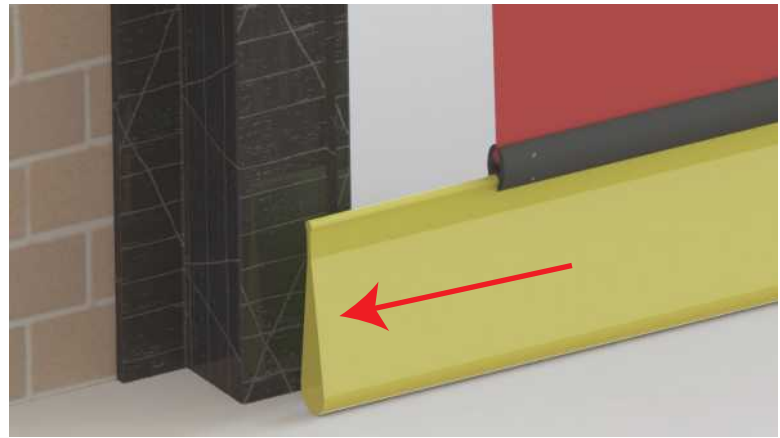
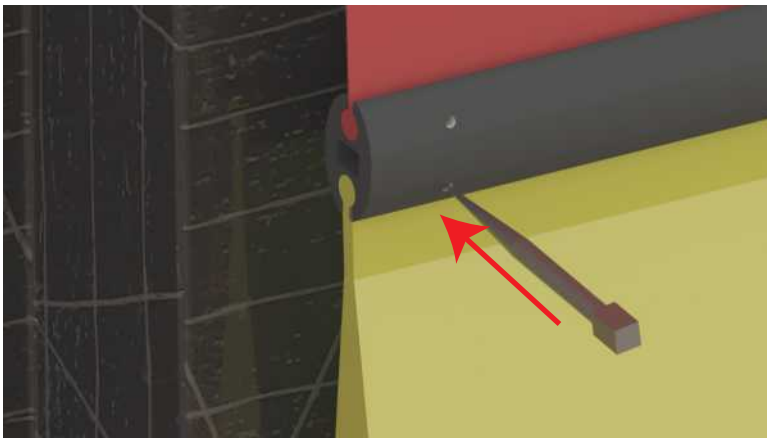
1. Pull down on the emergency release rope to disengage the motor.
2. Manually pull the door down to the closed position.
3. While holding the door in the closed position, pull the re-engage rope to lock the door closed.

XRS Section Replacement Instructions

1. Turn the power off to the control box.
2. Lift up on one side of the door until you can pull the black windbar out of the track.

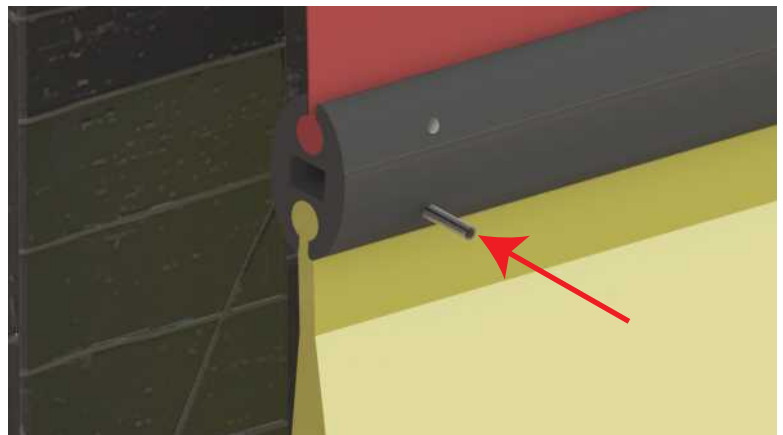


3. Once one side of the section is out of the track, repeat step 2 for the opposite side so the entire section and windbars are outside of the track.
4. To remove the section from the windbar, use a hammer and nail punch to remove the section retaining roll pin as shown below. Repeat for both sides of the section.
5. Please note that if you are replacing a section other than the bottom yellow section, you will have to remove a total of 4 retaining roll pins, one in each corner of the section.
6. Once the roll pins are removed, slide the section out of the windbar. See below.



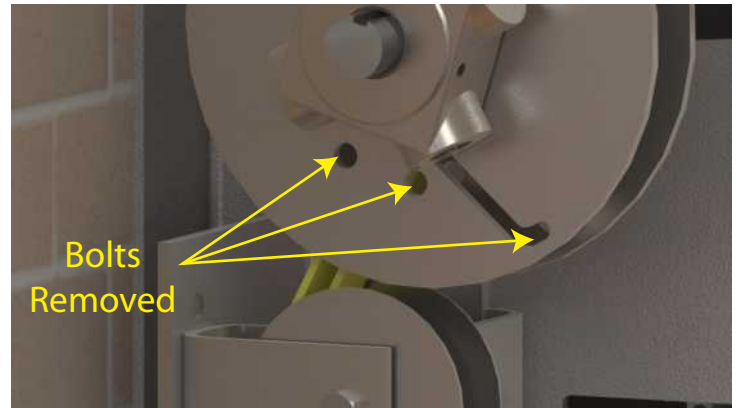
7. Once the section is in position, use a hammer to pound the roll pins back into position. If the roll pin does not want to pound through the section, you can use a 1/8" drill bit to drill a hole through the section first.

8. Turn the power back on and either lift the section back into the track one side at a time, or open the door with the motor and allow it to reset itself.

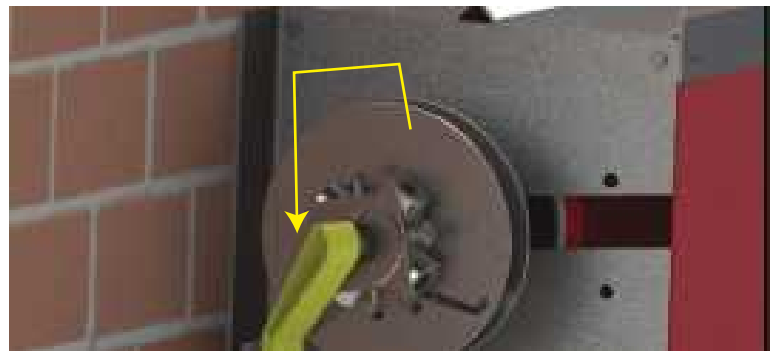
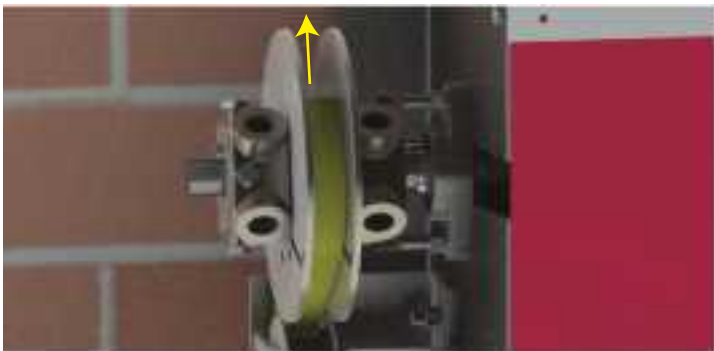


Counterweight Strap Replacement - Premium Model Only

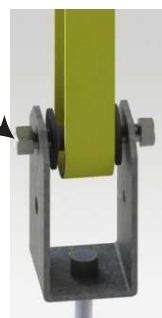
1. With the door in the closed position, remove the PVC guide tube by pulling out on the bottom of the tube and sliding it down off the weight stack.
2. Place the door in the Open position. ****CAUTION**** The weights will travel down as you open the door. Be sure no personnel are in the way and the weights will not hit or hang up on any obstruction.
3. Turn power off to the door control box.
4. Make sure the emergency release ropes are in the "engaged" position and the door is locked in place.
5. Remove the 3 bolts running between the stainless steel guide disks on the Strapeze assembly. See below.



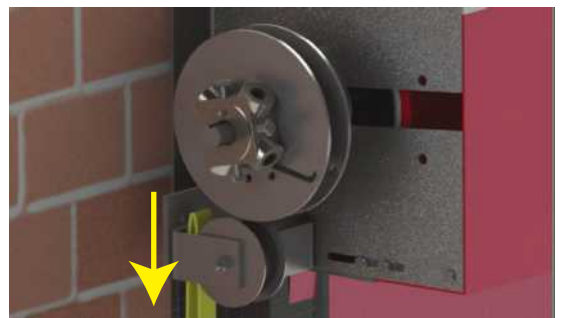
6. Lift the strap up from between the guide disks and slide it off the shaft.



7. Disassemble the strap clevis at the bottom of the strap that joins the weight stack to the strap.

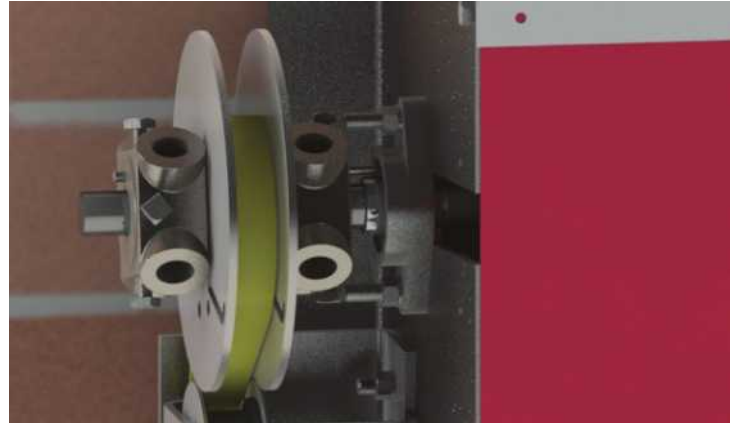
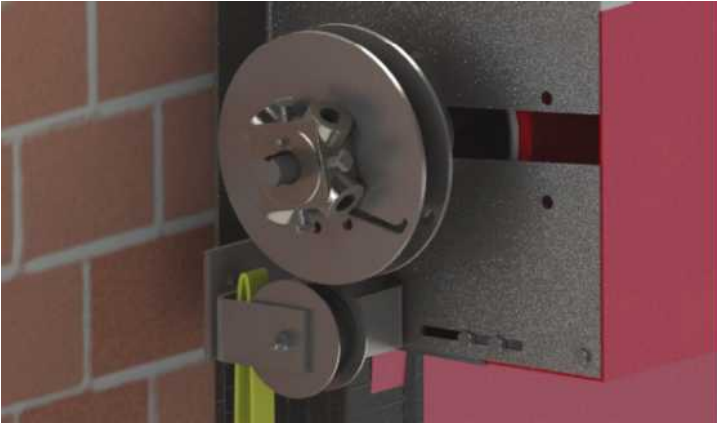


8. Slide the strap down and out from behind the strap pulley.

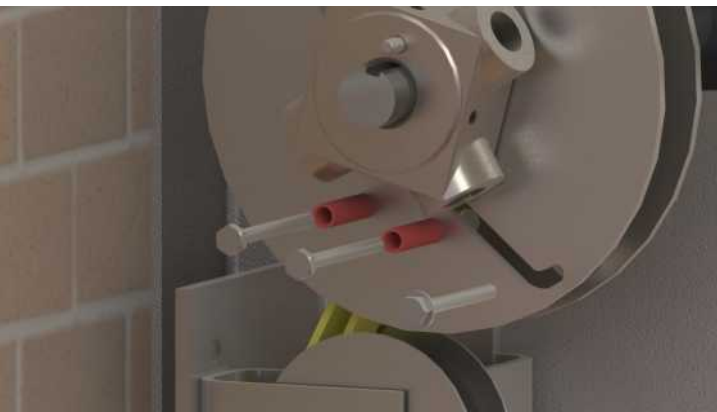


Strap Replacement Instructions cont.....

9. Slide the new strap up behind the strap pulley and loop it over the shaft in between the guide disks.



10. Insert the sleeves and bolts through the holes in the guide disks making sure the strap runs in between the sleeves as shown below. ****NOTE, the bolts going through the sleeves should be tight, but the bolt going through the "J" slot should remain loose so it can move freely..**



11. Reattach the strap clevis bracket to the bottom of the strap and restack the weights on the threaded rod.

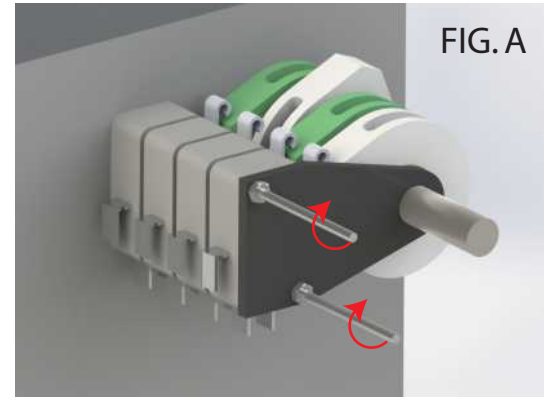


12. Use the small tubes of grease provided with the new strap to grease the entire length of the strap inside and out.

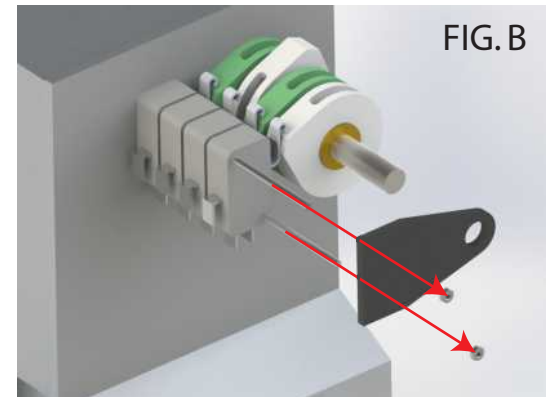
13. Re-install the PVC guide tube and guide tube bracket.

XRS Extreme Roll Up Series Replacement Limit Switch Instructions

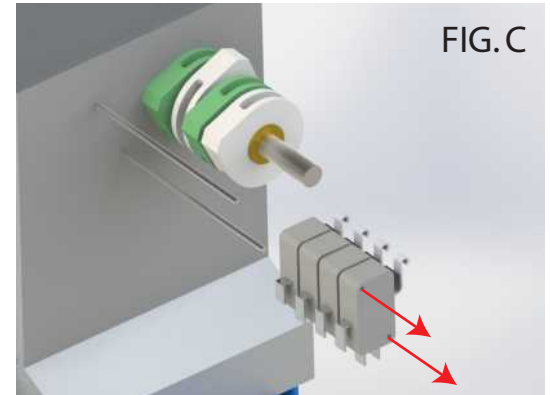
1. Use a pliers or vice grips to turn each retaining bolt slightly in the clockwise direction. This will allow you to then turn each bolt by hand and then pull out on the bolts to expose the nuts. (FIG. A)



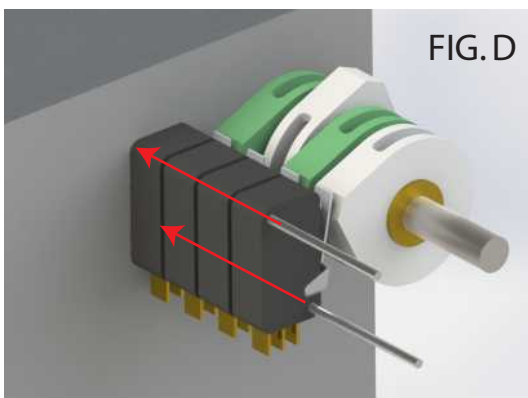
2. Once the bolts are loose enough to expose the nuts, unthread the nuts completely and slide the black plastic retaining plate off the bolts and cam shaft. (FIG. B)



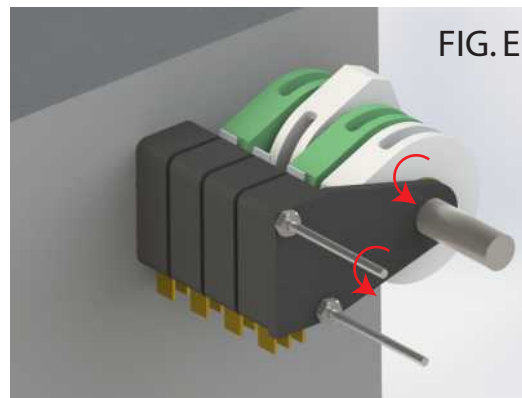
3. Note where the wires are attached to the existing limit switches. Next, remove the wires from the limit switch terminals and slide the 4 switches off the retaining bolts. (FIG. C)



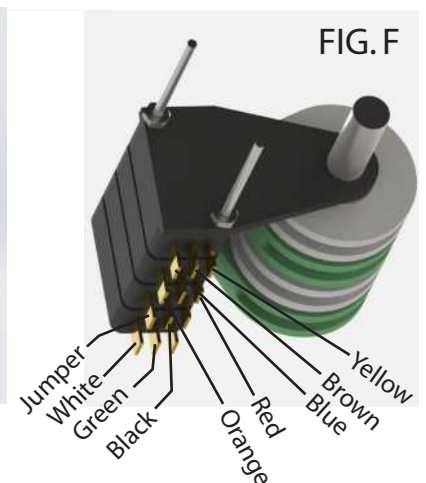
4. Slide 4 new limit switches onto the bolts making sure the lever side of the switches face the green/white cams. (FIG. D)



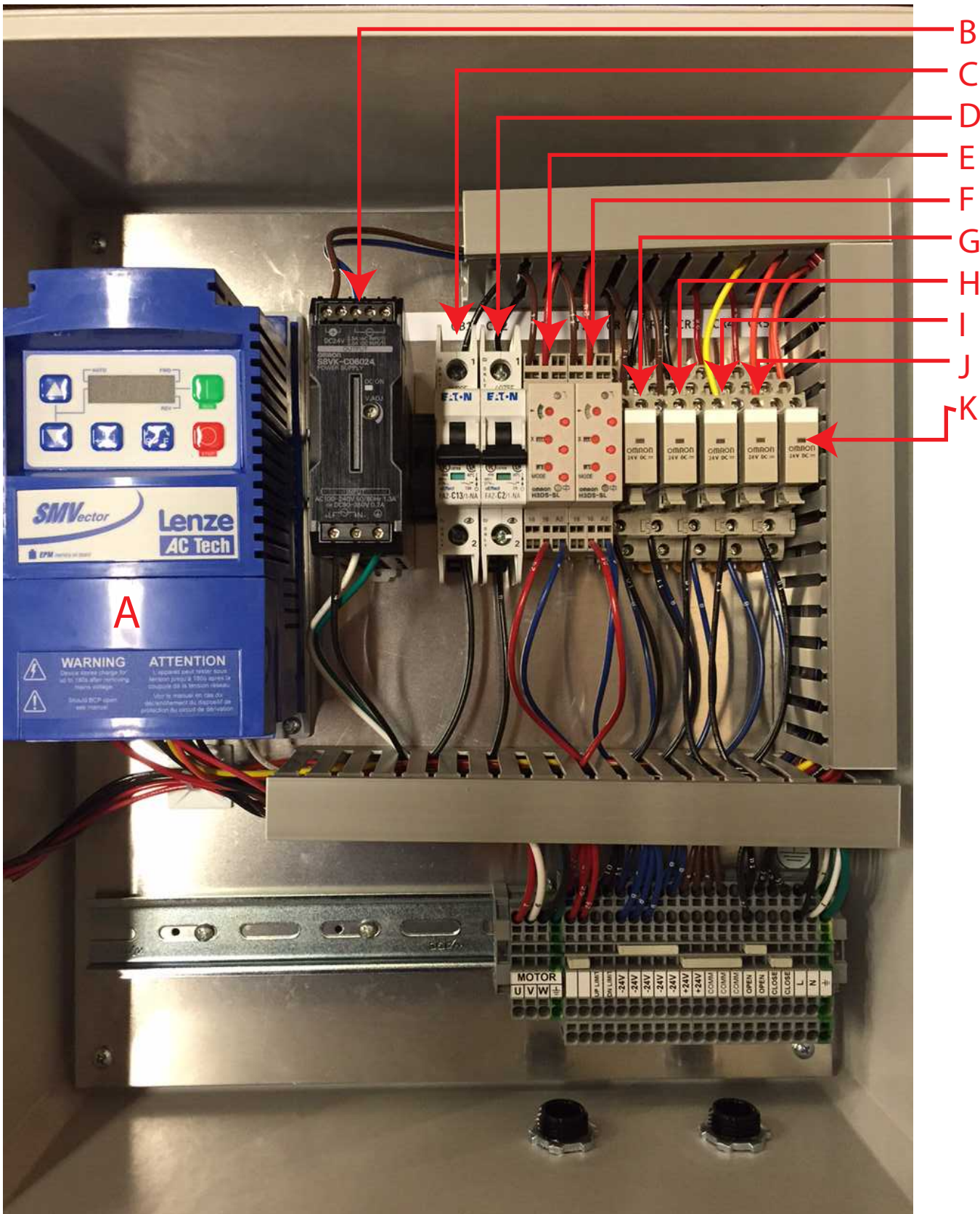
5. Slide the black plastic retaining plate over the bolts and the cam shaft. Thread the nuts onto the bolts until they are close to the plate. Push in on the bolts so the nut pushes into the recessed area of the plate. Rotate the bolts by hand counterclockwise until tight. (FIG. E)



6. Reattach the wires to the limit switches in the same position that they originally were. (FIG. F)



XRS Troubleshooting Control Components



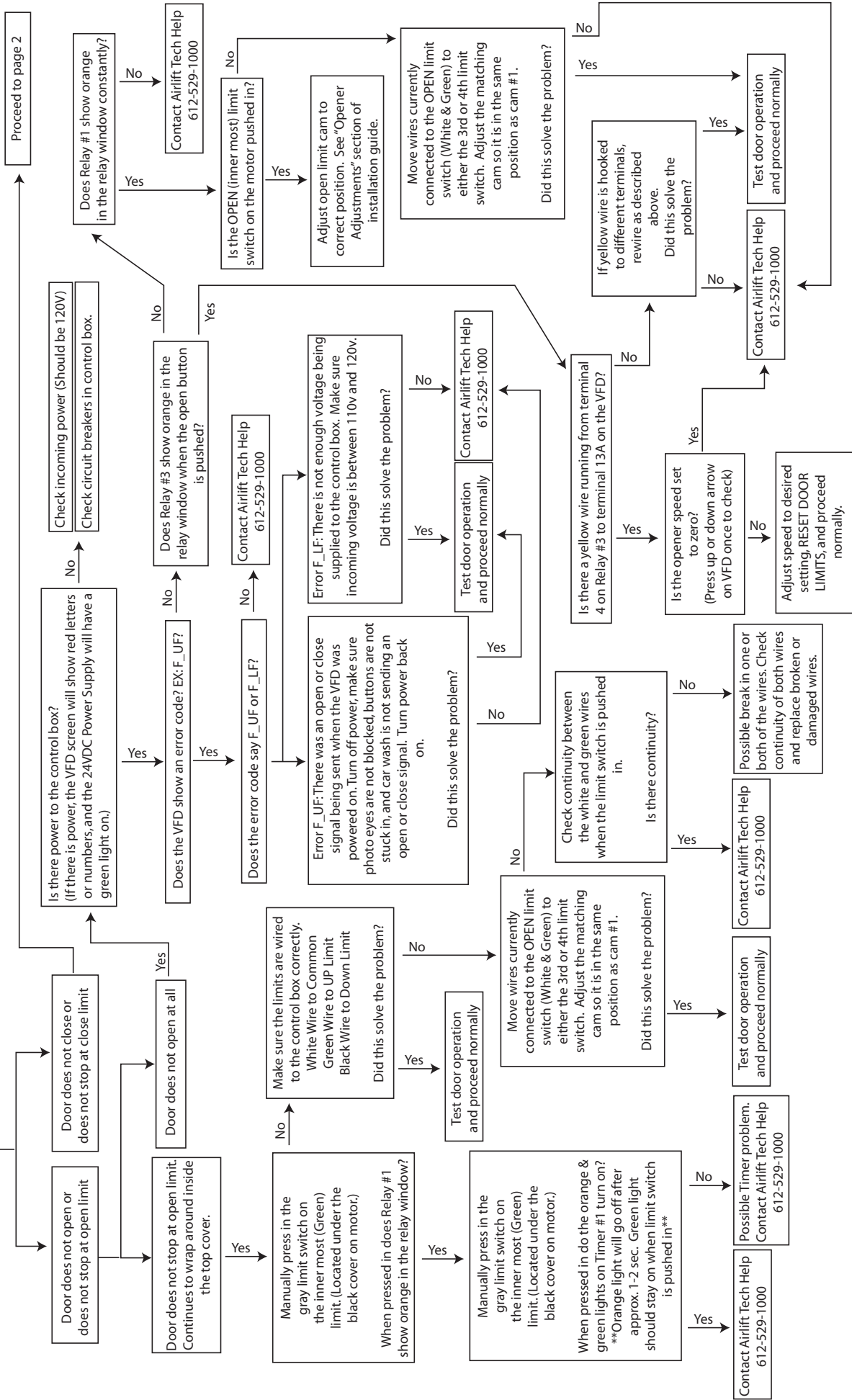
A: VFD Drive
B: 24VDC Power Supply
C: VFD Circuit Breaker
D: Control Circuit Breaker

E: Timer #1 (Open Limit Timer)
F: Timer #2 (Close Limit Timer)
G: Relay #1
H: Relay #2

I: Relay #3
J: Relay #4
K: Relay #5

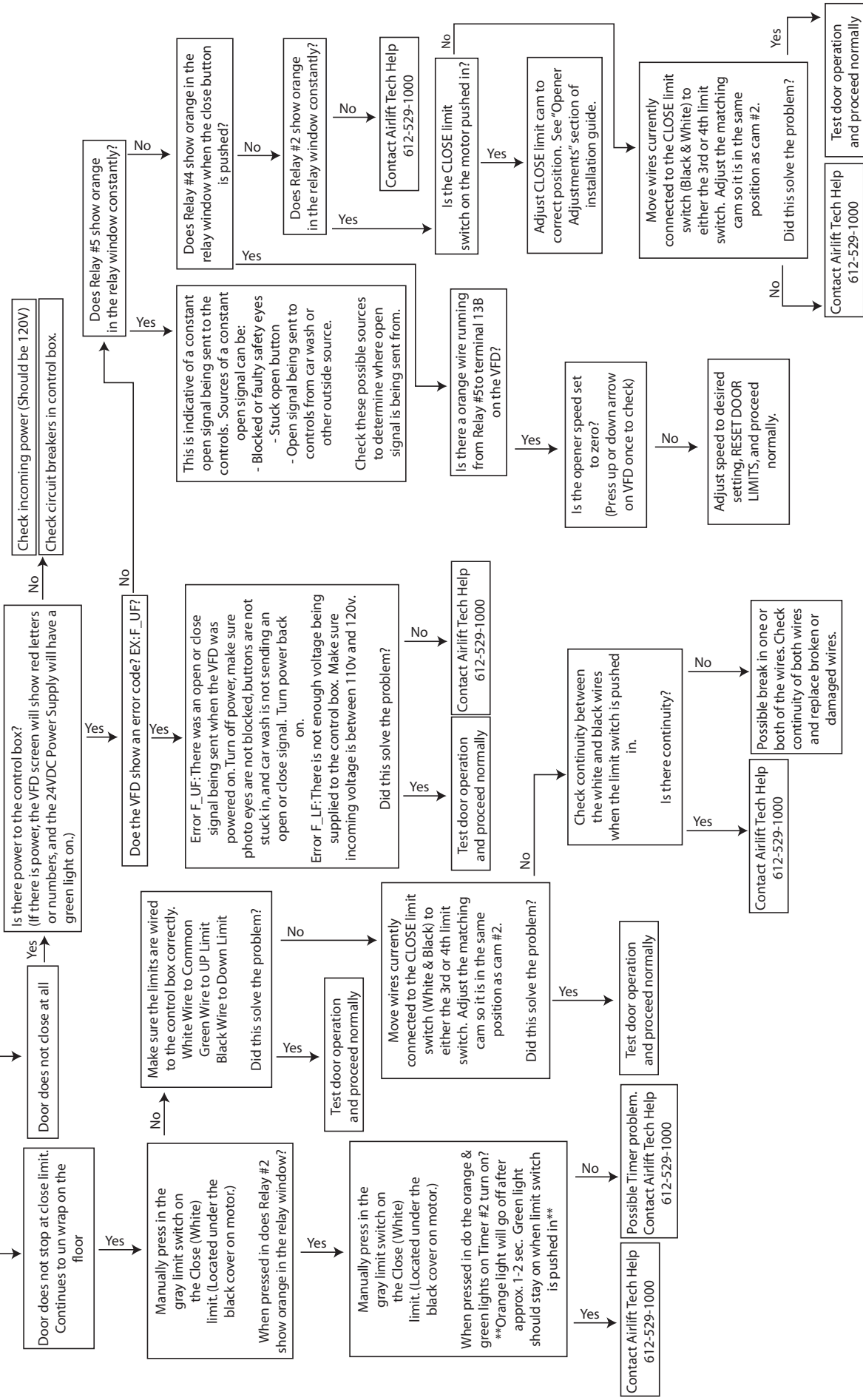
START HERE

XRS Troubleshooting (Page 1)



XRS Troubleshooting (Page 2)

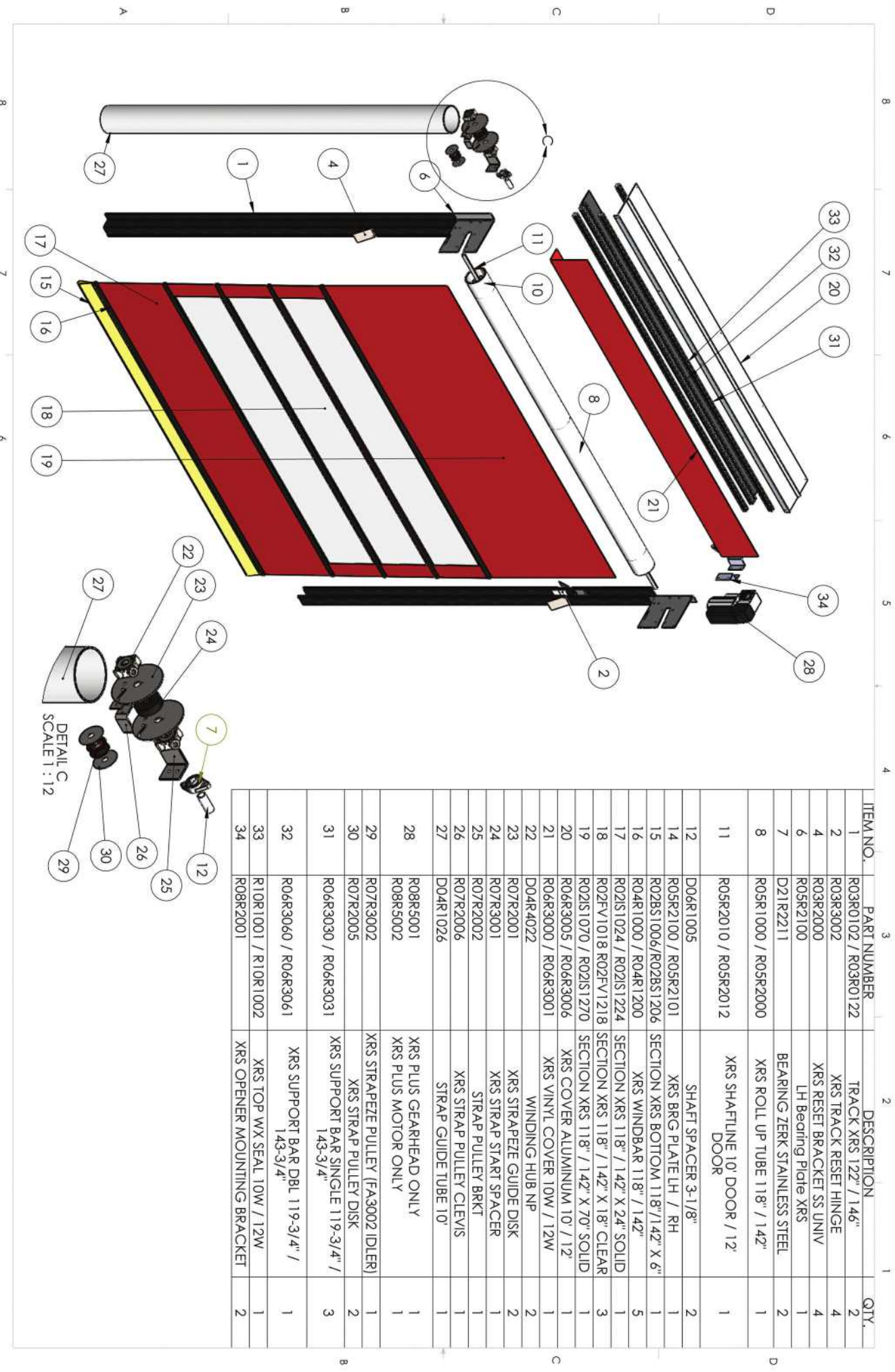
FROM PAGE 1



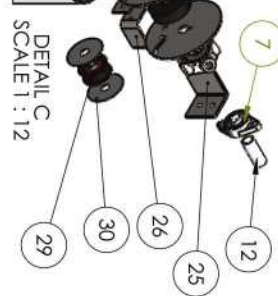
PAGE 3 - VFD ERROR CODE TROUBLESHOOTING

ERROR CODE	DESCRIPTION	POSSIBLE CAUSES	RESOLUTION
F.UF	START UP FAULT	Open or Close Signal present when power is turned on to control box.	Turn off power to control box and make sure safety eyes are not blocked, open button is not stuck in, car wash controls are not sending an open signal. Turn power back on. If error code is still present, contact Airlift Technical Assistance.
FLF	LOW INCOMING VOLTAGE	Incoming voltage is below 110VAC.	Determine reason for low incoming voltage. Supply voltage to control box must be 110v-120v.
F.CL or CL	CURRENT LIMIT EXCEEDED	<p>Motor drawing more than 8 amps due to:</p> <ul style="list-style-type: none"> - Improper adjustment of counterweight system or broken counterweight strap. - Door movement is restricted due to binding, ice, or other obstruction. - Excessive motor wear 	<p>Check counterweight system (Premium model only). With the door in the open position, the counterweight strap should be completely unwound with the weights towards the bottom of the guide tube. Check that the strap is not frayed or broken. Replace strap and/or adjust weights as necessary. Consult installation manual for proper weight adjustment.</p> <p>Make sure there are no loose bolts, worn bearings, ice, or any other obstruction preventing the door from traveling properly in the track. Also make sure door roll has not shifted to one side and is rubbing against either bearing plate.</p> <p>If counterweights and obstructions have been ruled out, the motor may be excessively worn. Consult with factory for troubleshooting.</p>
F.PF	MOTOR OVERLOAD (Preceded by F.CL)	See F.CL Possible Causes	See F.CL Resolution
FOF	OUTPUT FAULT	Short circuit to or in motor or Sever Motor Overload FOLLOW STEPS 1-8 TO THE RIGHT.	<ol style="list-style-type: none"> 1. Turn power off to control box and make sure wires leading to the two circuit breakers are not loose. If they are tight, move to Step 2. 2. Disconnect motor leads (U,V,W,Ground) from control box. 3. Turn power on and press Open or Close. If error persists, the VFD drive will need to be replaced. If error code goes away, go to Step 4. 4. Turn power off, rehook motor leads to control box. 5. Disconnect motor leads from the motor itself making sure none of the wires are touching each other or any part of the motor junction box. 6. Turn power on and press Open or Close. If error persists, there is a short in the motor lead wires and they should be replaced. If error goes away, go to Step 7 7. Turn power off, reconnect motor leads at the motor. 8. Turn power on and press Open or Close to run the door. If error persists, the motor will need to be replaced.
FOF1	GROUND FAULT ERROR	Grounded motor phase or short circuit to or in motor. FOLLOW STEPS 1-8 TO THE RIGHT.	

FOR ALL OTHER ERROR CODES, CONTACT AIRLIFT TECHNICAL ASSISTANCE @ 1-888-368-4403



ITEM NO.	PART NUMBER	DESCRIPTION	QTY.
1	R03R0102 / R03R0122	TRACK XRS 122" / 146"	2
2	R03R3002	XRS TRACK RESET HINGE	4
4	R03R2000	XRS RESET BRACKET SS UNIV	4
6	R05R2100	LH Bearing Plate XRS	1
7	D21R2211	BEARING ZERK STAINLESS STEEL	2
8	R05R1000 / R05R2000	XRS ROLL UP TUBE 118" / 142"	1
11	R05R2010 / R05R2012	XRS SHAFTLINE 10' DOOR / 12' DOOR	1
12	D06R1005	SHAFT SPACER 3-1/8"	2
14	R05R2100 / R05R2101	XRS BRG PLATE LH / RH	1
15	R02BS1006/R02BS1206	SECTION XRS BOTTOM 118"/142" X 6"	1
16	R04R1000 / R04R1200	XRS WINDBAR 118" / 142"	5
17	R02S1024 / R02S1224	SECTION XRS 118" / 142" X 24" SOLID	1
18	R02FV1018 R02FV1218	SECTION XRS 118" / 142" X 18" CLEAR	3
19	R02S1070 / R02S1270	SECTION XRS 118" / 142" X 70" SOLID	1
20	R06R3005 / R06R3006	XRS COVER ALUMINUM 10' / 12'	1
21	R06R3000 / R06R3001	XRS VINYL COVER 10W / 12W	1
22	D04R4022	WINDING HUB NP	2
23	R07R2001	XRS STRAPEZE GUIDE DISK	2
24	R07R3001	XRS STRAP START SPACER	1
25	R07R2002	STRAP PULLEY BRKT	1
26	R07R2006	XRS STRAP PULLEY CLEVIS	1
27	D04R1026	STRAP GUIDE TUBE 10'	1
28	R08R5001 R08R5002	XRS PLUS GEARHEAD ONLY XRS PLUS MOTOR ONLY	1 1
29	R07R3002	XRS STRAPEZE PULLEY (FA3002 IDLER)	1
30	R07R2005	XRS STRAP PULLEY DISK	2
31	R06R3030 / R06R3031	XRS SUPPORT BAR SINGLE 119-3/4" / 143-3/4"	3
32	R06R3060 / R06R3061	XRS SUPPORT BAR DBL 119-3/4" / 143-3/4"	1
33	R10R1001 / R10R1002	XRS TOP WX SEAL 10W / 12W	1
34	R08R2001	XRS OPENER MOUNTING BRACKET	2



XRS Plus Model Wiring Schematic

(See attached instructions for additional information)

Prewired "Pigtail"
Supplied By Airlift

**Yellow, Blue, Brown, Red, & Orange Wires are only used when additional inputs are required. If additional inputs regarding door position are needed please contact our technical support department for wiring instructions.

Green To Ground

Black To "W"

White To "V"

Red To "U"

Green To "UP Limit"

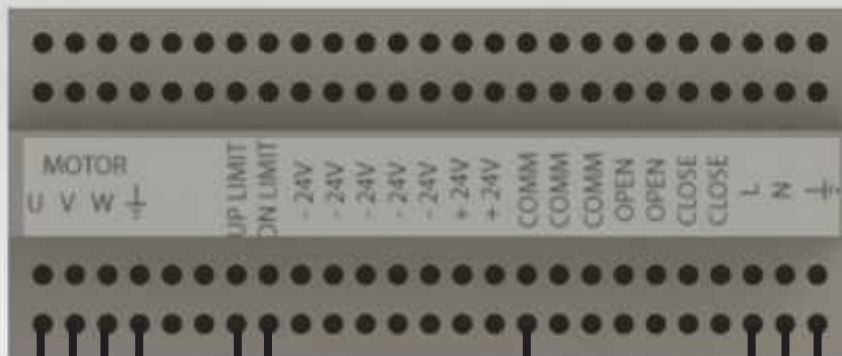
Black To "DN Limit"

White To COMMON

Terminal Block in Control Box

Please hook only one wire per terminal

**Motor Wiring Must Be
Minimum 12ga Stranded Wire
Supplied By Others**



Red

White

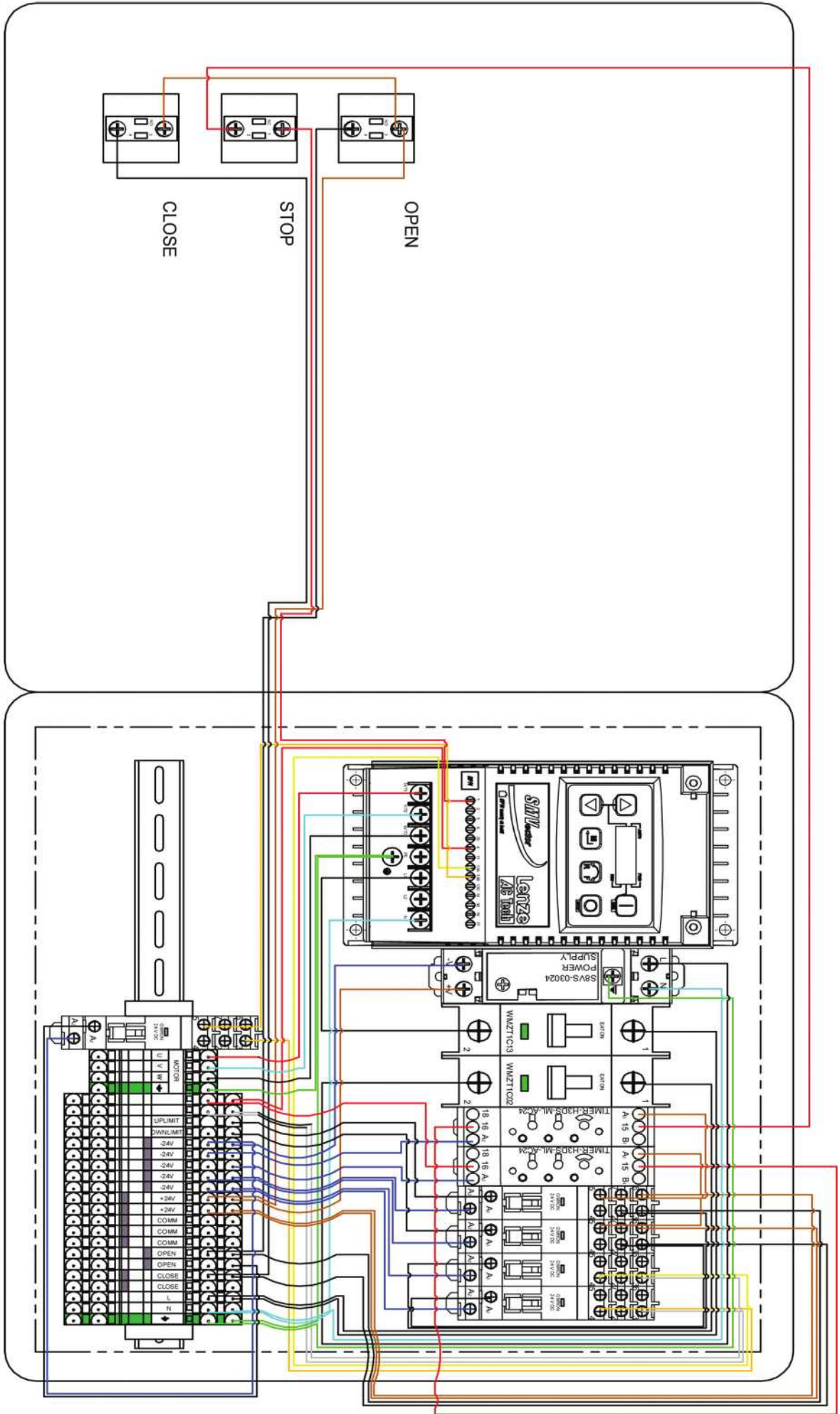
Black

Green

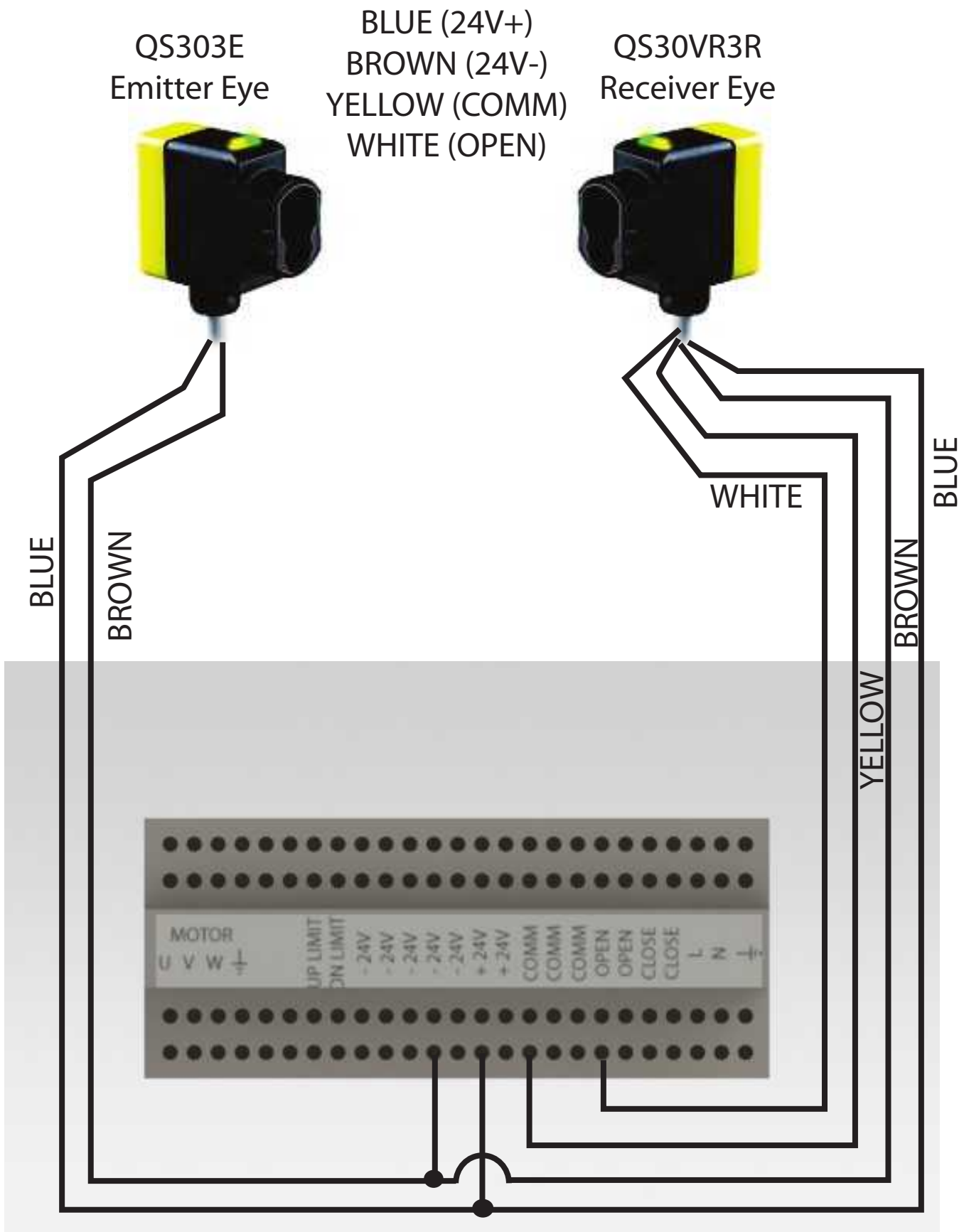
**120v Power requires
20amp circuit per
opener**

120v Ground
120v Neutral
120v Hot

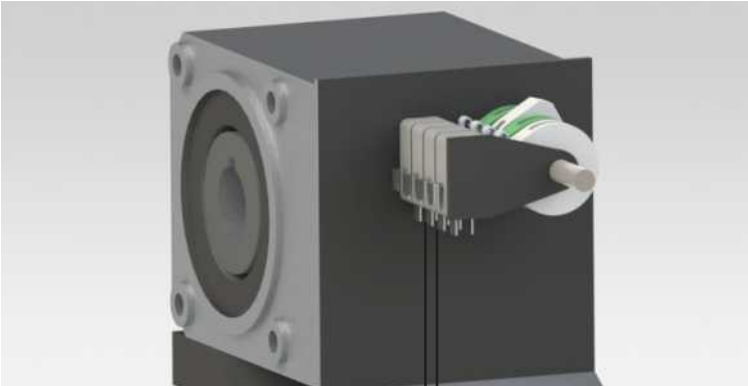
**Minimum 12ga Wire
Supplied By Others**



Banner Infrared Safety Eyes



Banner Infrared Safety Eyes With Limit Switch Shut Off



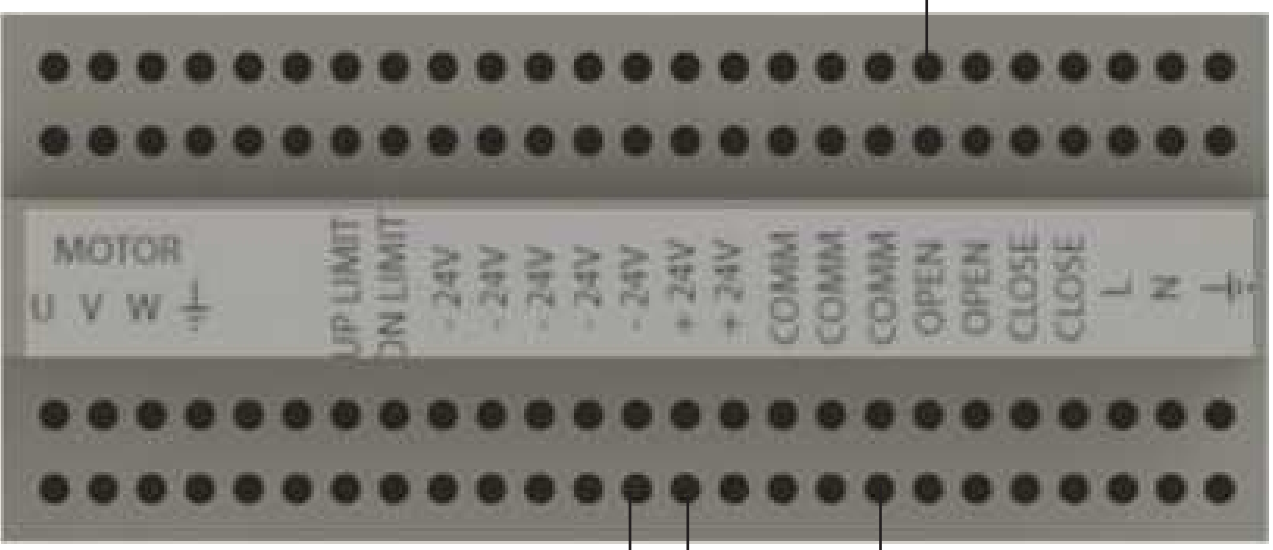
BLUE (24V+)
BROWN (24V-)
YELLOW (COMM)
WHITE (TO ORANGE FROM LIMITS)
RED (FROM LIMITS TO OPEN)
**ADJUST OUTER GREEN CAM
TO MATCH WHITE CLOSE LIMIT CAM.

ORANGE WHITE

RED

BLUE

BROWN



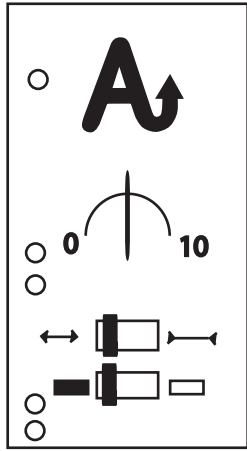
YELLOW

BROWN

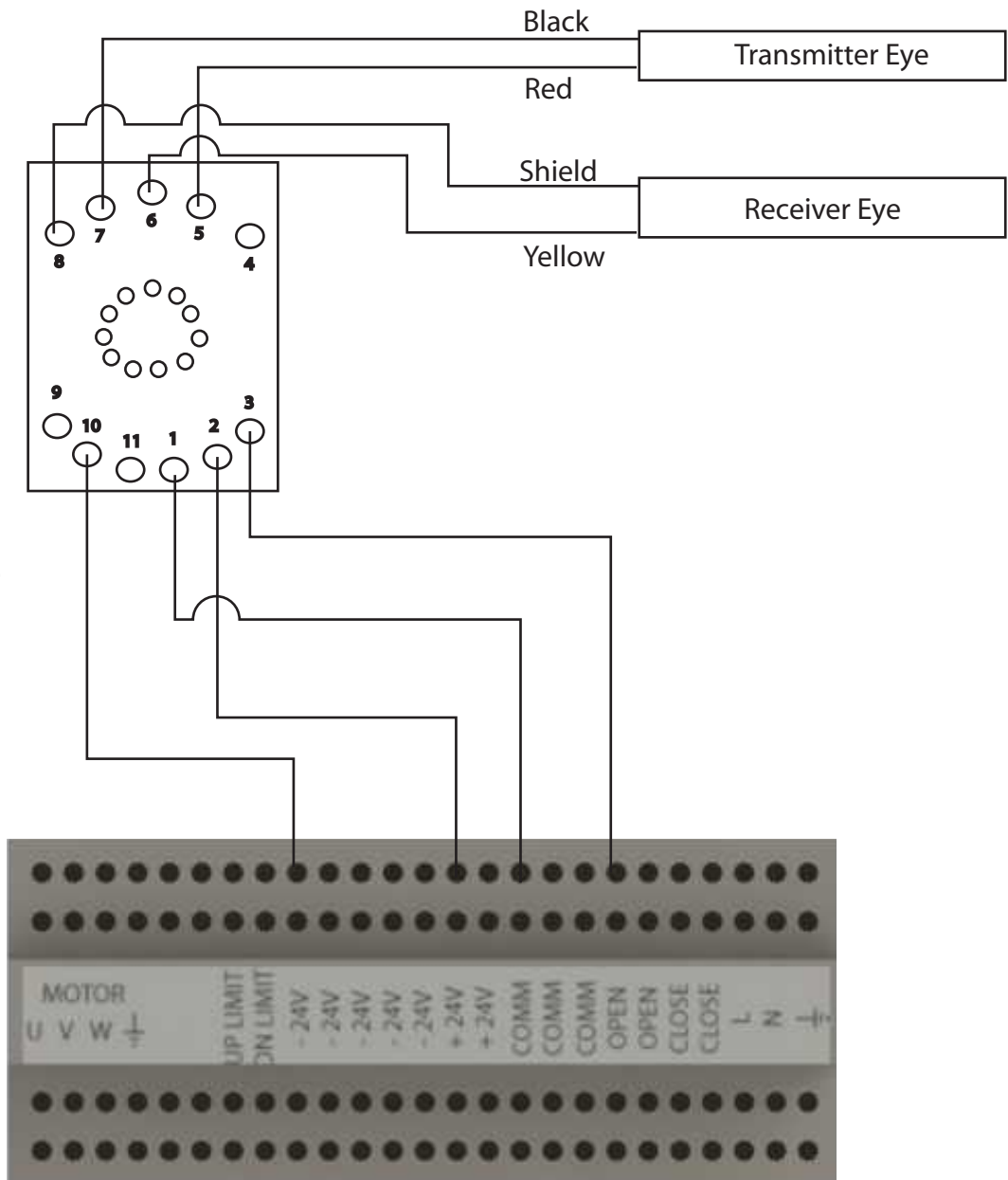
BLUE

XRS Plus Telco Photo Eye Wiring

Telco Amplifier



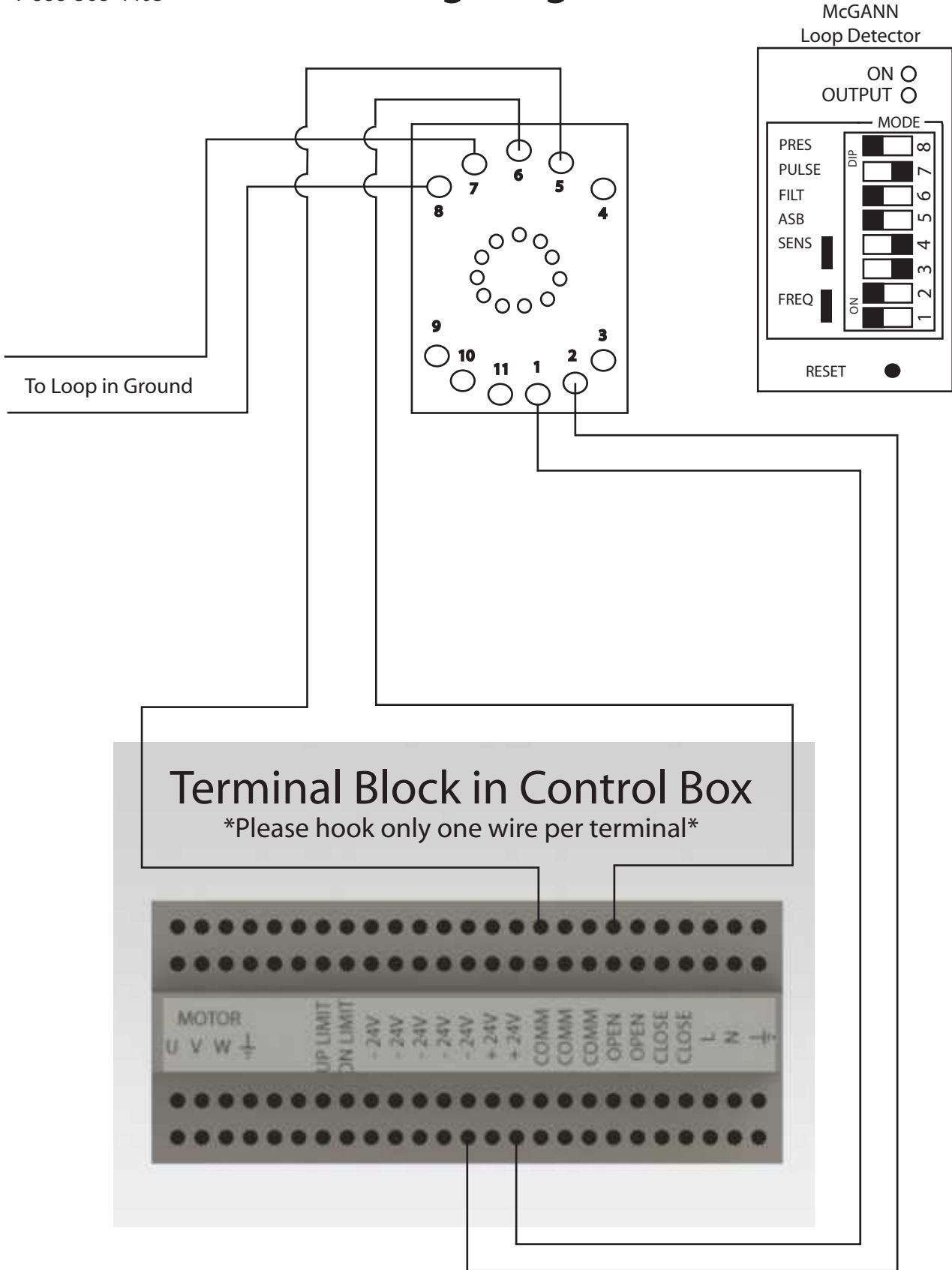
BOTH DIP SWITCHES
NEED TO BE IN THE
LEFT POSITION





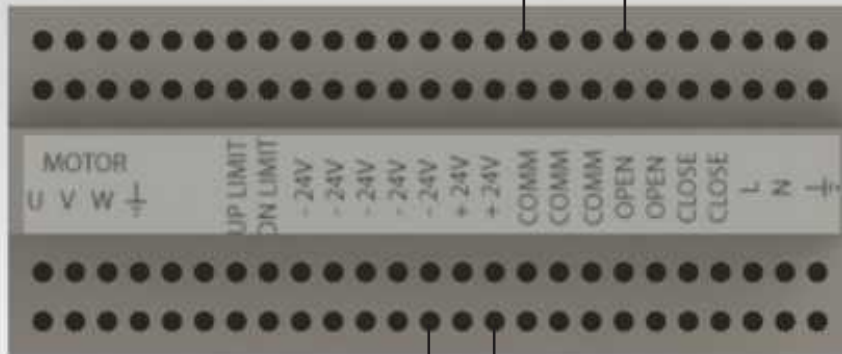
Airlift Doors, Inc.
 4700 Osseo Rd.
 Minneapolis, MN 55430
 (612) 529-1000
 1-888-368-4403

XRS Plus Loop To Open Wiring Diagram



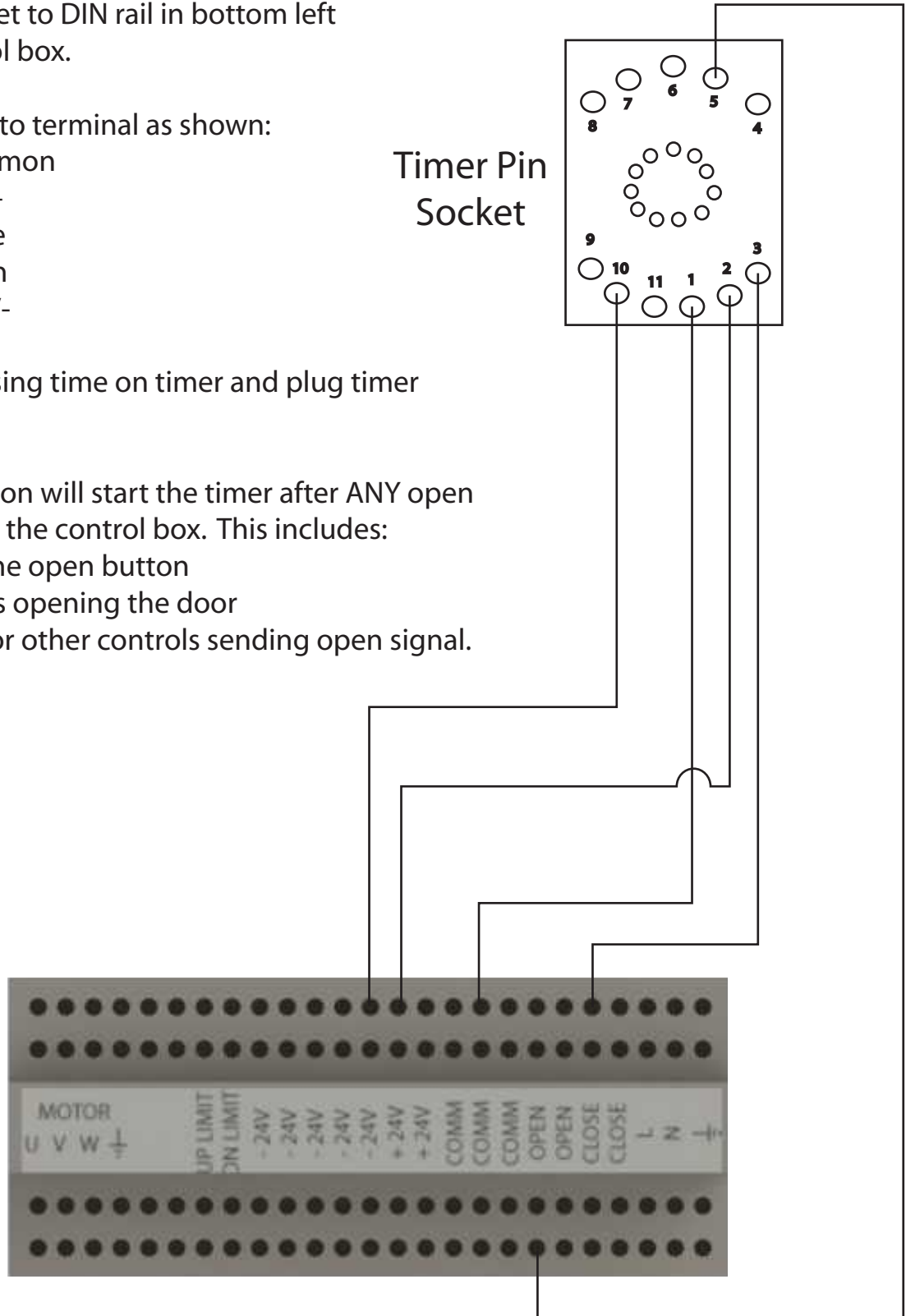
Terminal Block in Control Box

Please hook only one wire per terminal



XRS Plus Timer to Close Wiring Macromatic timer

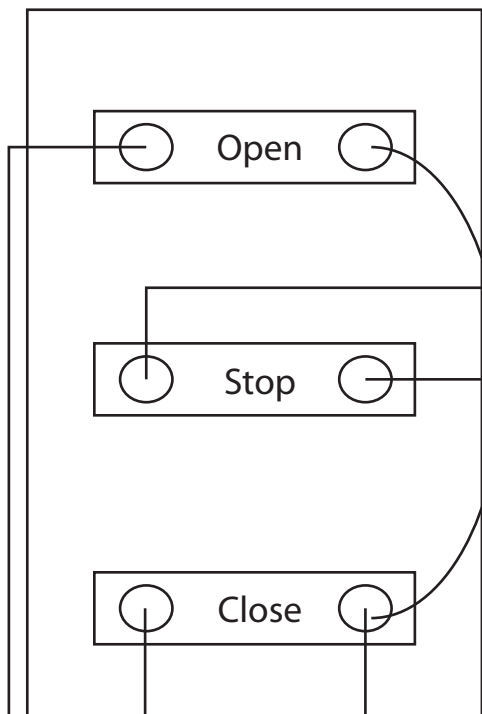
1. Mount pin socket to DIN rail in bottom left corner of control box.
2. Wire pin socket to terminal as shown:
 - #1 to Common
 - #2 to 24V+
 - #3 to Close
 - #5 to Open
 - #10 to 24V-
3. Set desired closing time on timer and plug timer into pin socket.
4. This configuration will start the timer after ANY open signal is sent to the control box. This includes:
 - Pressing the open button
 - Photo eyes opening the door
 - Car wash or other controls sending open signal.



XRS Plus Model 3 Button Station Wiring

VFD Control

3 Button Station



Jumper wire
between open
and close
contact blocks
DO NOT JUMP
TO STOP CONTACT

New wire run back
from STOP contact
block to terminal #1

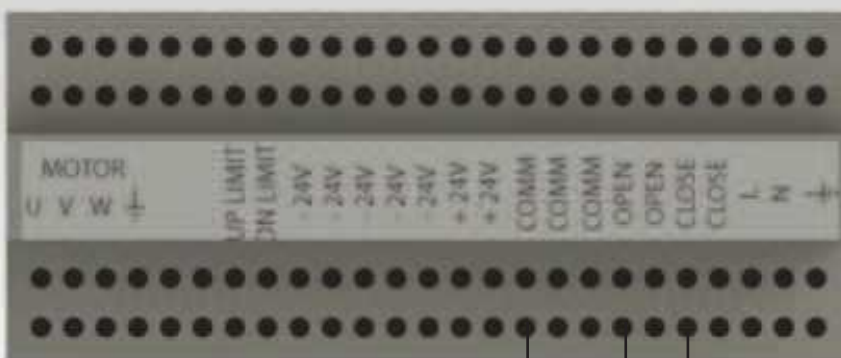
Remove red wire
from terminal #1
in VFD and wire to
either terminal on
STOP contact block
in 3 button station

Red wire removed from terminal #1



Terminal Block in Control Box

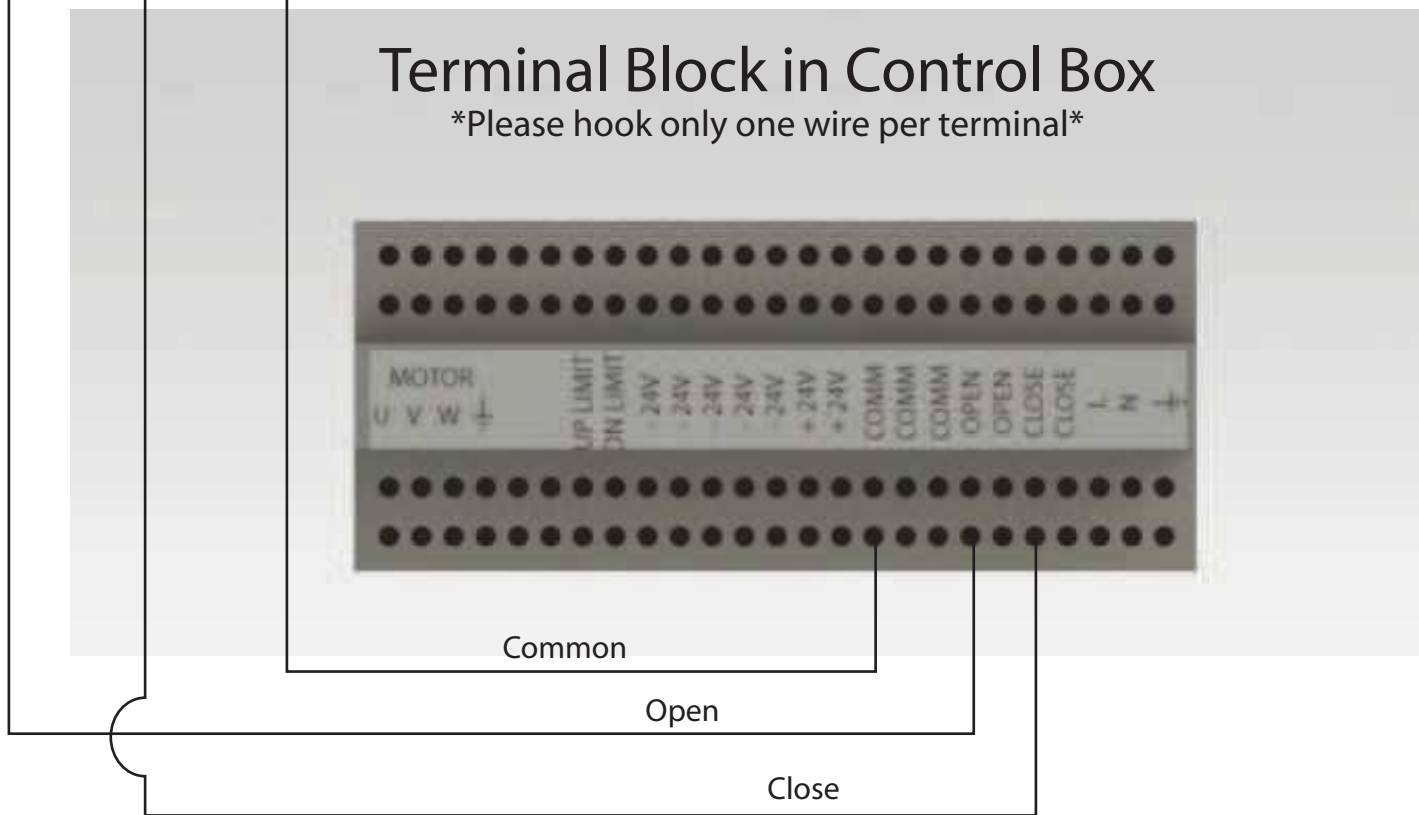
Please hook only one wire per terminal

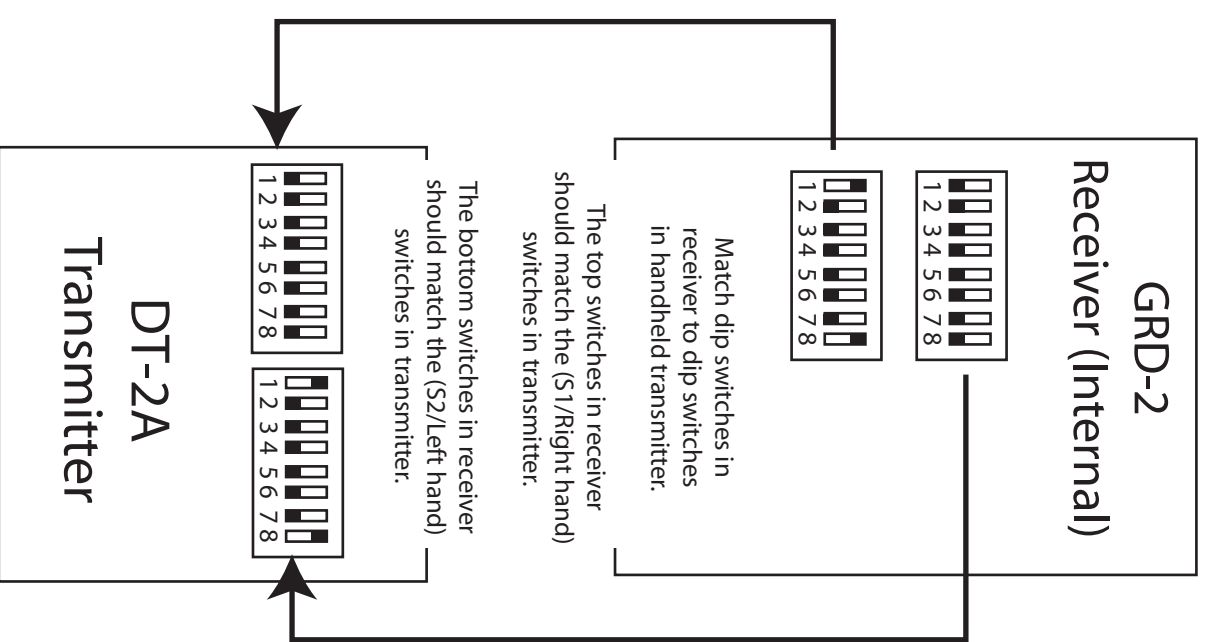
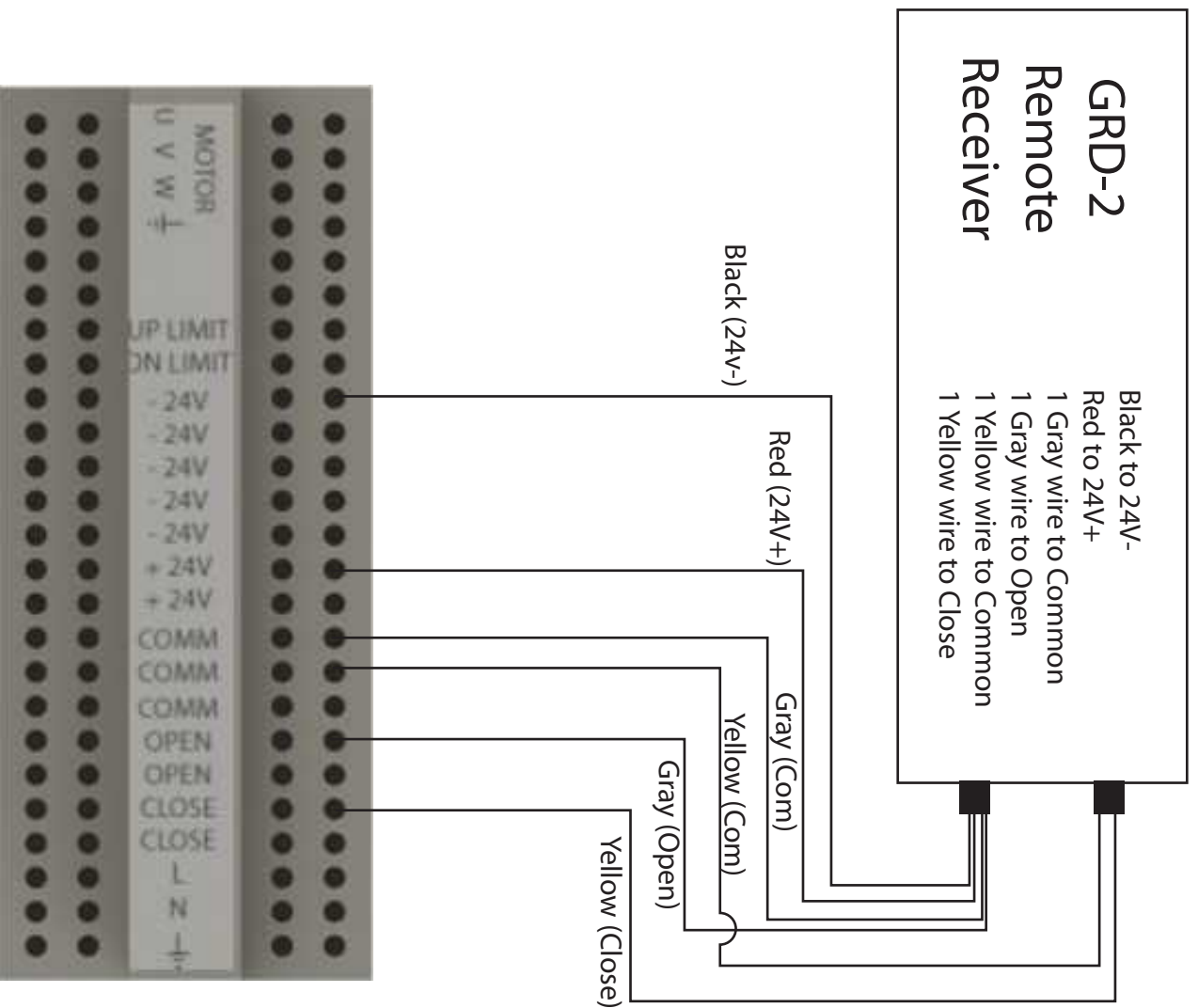


Common

Open

Close





Airlift Doors, Inc.
 XRS Extreme Roll Up Series
 GRD-2 Remote Control Wiring



AIRLIFT DOORS, INC.

QUARTERLY MAINTENANCE PROGRAM XRS ROLL UP DOOR

Airlift Doors, Inc. recommends performing quarterly maintenance on your XRS roll up door.

Maintenance steps explained:

A. Grease Zerk Bearings

Grease all zerk bearings located on the shaftline with lithium grease. Use a grease gun and attach to the zerk and pump with grease until the grease begins to push through the bearing. Perform this step quarterly to ensure proper lubrication and operation of the shaftline.

B. Weatherstrip Inspection

Check all brush seal or vinyl seal on the vertical tracks. Note: The seals are on the inside and outside of the door. Check for any missing or damaged portions and anything that would allow air gaps. Any damaged portions should be replaced. Check top rubber seal along the door to make sure it is not falling out and is still sealing against the vinyl of the door. Weatherstrip should be inspected quarterly.

C. Strap Inspection (Premium Model Only)

Perform a visual inspection of the counterweight strap for any wear or signs of fraying. You may need to remove the PVC guide tube to get full view of the strap. Consult strap replacement instructions in this manual for further instruction on removing the tube. If the strap shows signs of wear or fraying it should be replaced. Inspect strap quarterly.

D. Test Emergency Release (Premium Model Only)

Pull the emergency exit cord attached to the motor. The motor should disengage and the door should open. If the door does not open or the motor will not disengage, consult factory for recommended solution to the problem. Make sure you re-engage the motor by pulling the opposite rope after testing. Perform this step quarterly.

E. Cleaning

Clean all vinyl sections of the door to eliminate buildup of chemical, water spots, mold, and mildew. To clean spray entire door with a mild soap or Airlift vinyl cleaner (part number D24R1007). Use a sponge or soft cloth to wash the door. Spray with high pressure rinse to remove all soap and chemical from the door. This step should be performed quarterly or as needed.

F. Safety Eyes

Operate motor with open and close buttons. Test photo eyes by obstructing the photo eyes when the door is closing to see if the door reverses instantly upon obstruction. Do not stand under the door while performing this step. Test eyes quarterly. Refer to the troubleshooting area of this manual or consult your service provider if eyes are not working properly.

Quarterly Maintenance Program For XRS Roll Up Door

Year: _____

Note: Mark date in each box when maintenance is performed to keep accurate records

<u>Steps</u>	<u>1st Quarter</u>	<u>2nd Quarter</u>	<u>3rd Quarter</u>	<u>4th Quarter</u>
A. Grease Zerk Bearings				
B. Weatherstrip Inspection				
C. Strap Inspection (Premium Model Only)				
D. Emergency Pull Rope (Premium Model Only)				
E. Cleaning				
F. Safety Eyes				

1st Quarter Notes:

2nd Quarter Notes:

3rd Quarter Notes:

4th Quarter Notes:

Limited Warranty for XRS Vinyl Roll Up Doors

Airlift Doors, Inc. warrants to the original purchaser or original user that all XRS model doors sold by Airlift Doors, Inc. and all parts thereof are free from defects in material or workmanship under normal use and service. Airlift Doors Inc. sole obligation under this warranty shall be limited to furnishing replacement parts F.O.B Maple Lake, Minnesota for the periods specified below from the date of initial shipment by Airlift Doors, Inc.

Panels	(3) Year
Windbars	(3) Year
Track	(3) Year
Motor	(1) Year
Gearbox	(1) Year
Control Box & Components	(1) Year
Photo Eyes	(1) Year

These Warranties are void if the original product warranted has been damaged by accident, abuse, misuse or neglect, improper installation or service, unauthorized modifications, misapplication, or other use not arising out of defects in material and workmanship.

Warranty redemption requires verification of original purchase date and completion in full of return goods form. Returns are only accepted when return authorization number has been provided by Airlift Doors, Inc. before product is returned.

The warranties set out in this certificate are the exclusive remedy of the original owner or user in lieu of all other warranties written, oral or implied (including any warranty or merchantability or fitness for the purpose) and all other obligations or liabilities on the part of Airlift Doors, Inc. Airlift Doors, Inc. neither assumes nor authorizes any person to assume for it any other obligation or liability in connection with the sale, installation, or use of the XRS Door or any parts thereof.

Airlift Doors, Inc. will not be responsible for labor or shipping and handling charges for the analysis of a defective condition or for the replacement and installation of defective parts.

The warranties herein shall be null and void if the XRS Door is not installed by a competent contractor and/or if the XRS Door is not installed according to Airlift Doors, Inc. provided instructions.

The warranties herein shall not cover claims of condensation due to normal temperature changes.

In no event will Airlift Doors, Inc. be responsible for, or liable to anyone for, special, indirect, collateral, punitive, incidental, or consequential damages, even if Airlift Doors, Inc. has been advised of the possibility of such damages. Such excluded damages include, but are not limited to, personal injury, damage to property, loss of goodwill, loss of profits, loss of use, cost of cover with any substitute product, interruption of business, or other similar indirect financial loss.



AIRLIFT DOORS, INC.

RETURN GOODS AUTHORIZATION

Airlift Doors, Inc. will not accept any return goods without prior authorization. Only the original purchaser is authorized to obtain return approval. If the goods were not purchased directly from Airlift Doors, Inc. please contact the Company the goods were purchased from to process the return with Airlift Doors, Inc.

Return Goods Procedure Directions:

Please complete this form and fax it to 612-588-7660 or e-mail to info@airliftdoors.com. Upon review and approval of this form you will be contacted by your preferred contact method.

Company: _____

Address: _____

City: _____ State: _____ Zip: _____

Contact: _____

Preferred method of Contact: _____

Original Invoice #: _____

Item(s) being returned: _____

Part # (if known): _____

Reason for Return: _____

Return Terms and Conditions:

All pre-approved returns must have the RGA number printed on the outside of the package

All Returns must be made within 30 days of authorization.

A copy of the RGA Slip and packing slip must be included with the return when replacement items have been shipped. Airlift Doors, Inc. will provide this slip with the replacement shipment.

All items to be returned to:

Airlift Doors, Inc

RGA # _____

400 State Hwy. 55

Maple Lake, MN 55358